

Walsall Council
Walsall Market
Design and Access Statement

ARP-WAL-DAS

Planning | 15 December 2014

This report takes into account the particular instructions and requirements of our client.

It is not intended for and should not be relied upon by any third party and no responsibility is undertaken to any third party.

Job number 232986

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1 Introduction

1.1 Design and Access Statement

Arup has been commissioned by Walsall Council (WC) to develop a two-phase scheme for a new permanent open market area within Walsall town centre, comprising of improvements to the existing urban realm. This new market is intended to replace the existing market operating in the Town Centre, with a higher quality consolidated scheme, which seeks to improve the long term success of both the market and the Town Centre as a whole.

This new market will be located within the Bridge Street Conservation Area, which is already utilised on a regular basis for parts of the existing market. The scheme will be developed over two consecutive phases to minimise disruption to the traders. As part of the application the following Design and Access Statement (DAS) has been written to explain the design principles and concepts that have been applied to the development. Reference should also be made to the other planning documentation, which is summarised within the Planning Statement.

1.2 Structure of Document

The DAS is structured in accordance with CABE¹ guidance and is set out as follows:

- Section 2 Assessment Context – Consideration of the physical, social, economic and policy issues surrounding the site and its development.
- Section 3 Involvement – Discussion on the involvement of groups and people in developing the proposal presented in this DAS.
- Section 4 Evaluation – Opportunities and constraints identified and how they have been resolved.
- Section 5 Design – The story of how the scheme has reached the application stage, focusing on the use, amount, layout, scale, landscaping and appearance of the proposal.
- Section 6 Access – Discussion on general movement to and through the development and how all members of society can use it.

1.3 The Application Site

The proposed site as illustrated on plan APR-PL-001 includes pedestrianised areas at The Bridge, lower Park Street, High Street/Digbeth, Bradford Street and Lower Hall Lane, within Walsall Town Centre. The site covers an area of 0.885ha, as identified on the red line boundary plan drawing reference ARP-PL-001.

¹ Commission for Architecture and the Built Environment, 2006, Design and Access Statements How to write, read and use them.

The scheme is to be developed in two consecutive phases:

- Phase One consists of the start of public realm works with the removal of six existing trees in The Bridge area (to be replaced in Phase Two) to facilitate the relocation of eleven stalls and one catering vehicle on The Bridge. These relocations will also require the existing Hippo sculpture to be temporarily relocated to the Civic Quarter, and be reinstated in Phase 2.

In addition to the stall relocations in The Bridge area, it is proposed to relocate up to six stalls into Market Square and two stalls into Bradford Street in addition to the existing stalls in these locations. These relocations are necessary in order to implement the Exclusion Zone in Digbeth and to facilitate the Network Rail Electrification Works taking place in the Town Centre, in 2015. The need for Phase One is explained in more detail in Section 3 of this document.

- Phase two will establish new stalls within The Bridge area and lower Park Street, the removal of all existing stalls from Bradford Street and Market Square and completion of the remaining public realm works on The Bridge, Digbeth and lower Park Street. No public realm works are proposed for Bradford Street within this application other than at the junction of The Bridge.

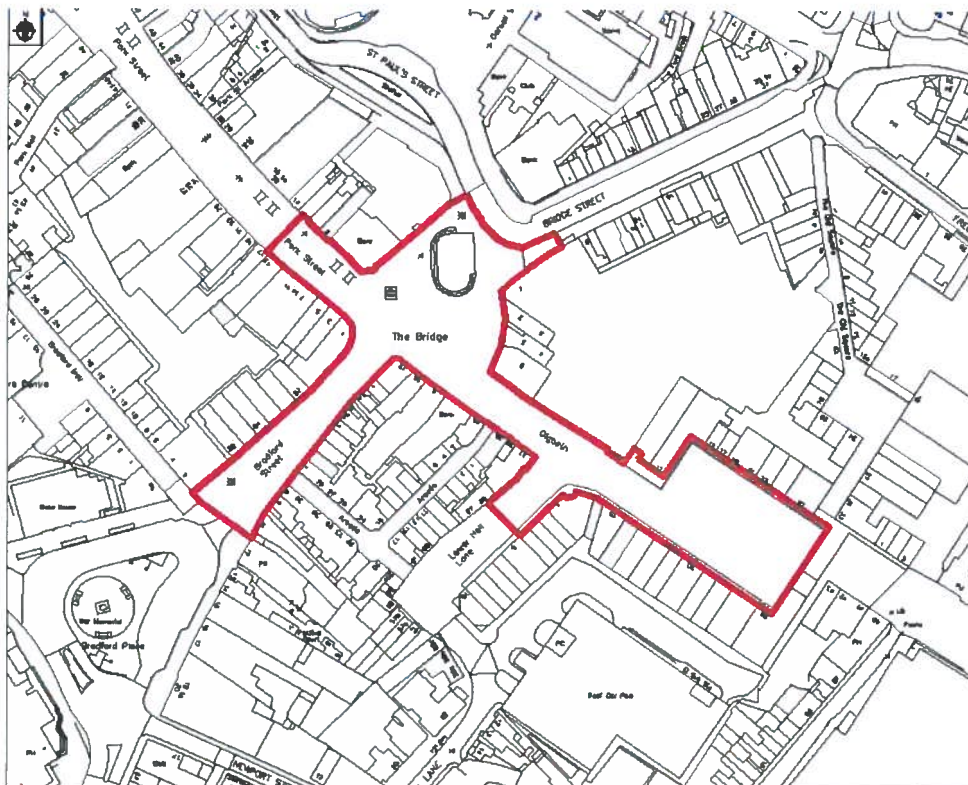


Figure 1 - Planning Application phased red line boundary.

The site comprises a prominent position along key pedestrian shopping routes, whilst The Bridge area provides a large public square connecting Park Street and Bridge Street. The pedestrian routes are lined by shops and financial/professional services within the historic core of the town centre. The site is within the Bridge Street Conservation Area, with several listed buildings. This includes a range of impressive frontages with Lloyds Bank at the corner of The Bridge and lower Park Street and the Victorian Arcade to the south of Park Street off Digbeth.

The area is bound by large redevelopment areas to the north and south, including the Old Square Shopping Centre (planning reference 12/1666/FL) and the St Matthews Quarter retail development (planning reference 13/1421/FL).

The St Matthews Quarter retail development will provide a new retail frontage along the southern side of High Street and the eastern side of Lower Hall Lane. The development will provide 11 units, mostly shops (A1) with some financial/professional services (A2) and food places (A3 and A5) with a total floor space of 8,257m².

To the north the redevelopment of the Old Square Shopping Centre will create a large retail presence, with flagship stores including a Co-operative food store fronting The Bridge and Primark fronting Digbeth.

The site is within walking distance of Walsall Railway Station to the west, Walsall Bus Station to the north west and several bus stops and car parks. Dedicated cycle routes also provide good access to and through the town centre.

The site forms a large part of the area of the current Walsall Market, which was granted a temporary permission until April 2015 (planning reference 13/0433/FL). The Bridge area includes twelve semi mature Tilia (common lime) trees, a raised podium area and several prominent works of public art including the fountain, the Sister Dora monument and the Hippo sculpture.

The site is entirely within the Primary Shopping Area, as defined by the Walsall Local Plan and identified in the emerging Walsall Town Centre Area Action Plan.

1.4 The Proposal

The application proposes a two phased approach to the scheme Phase One will include the positioning of up to 76 stalls and 4 catering vehicles in lower Park Street, The Bridge, Bradford Street and Market Square, until Phase Two is implemented. Phase One also includes the removal of 6 trees in south-eastern area of The Bridge and the relocation of the Hippo sculpture to the Civic Quarter, both to be reinstated on the Bridge in Phase Two.

Phase Two will include the permanent location of up to 70 market stalls and two dedicated catering vehicles at The Bridge and lower Park Street and removal of all existing stalls from Bradford Street and Market Square. The market stalls will be arranged to allow flexibility in the use of either 3x3m or 3x4.5m stalls in a number of positions. Refer to planning drawings ARP-PL-020 and 021.

The Bridge area will be improved to create a more attractive public square, with sufficient space to accommodate the market stalls. This will be enabled by the permanent removal of the podium and repositioning of the fountain, The Source of Ingenuity, to a site within Walsall yet to be determined. The consideration of a new location for the fountain will be subject to a separate piece of work to be undertaken by Walsall Council and will focus on finding a suitable alternative town centre location.

The Sister Dora monument, Hippo sculpture and clock will be relocated to new positions within The Bridge area. The extent of this relocation is shown on drawing ARP-PL-040.

Consolidating the market stalls within The Bridge, a key civic space, with effective boundary treatment will achieve a vibrant market place. The stalls on lower Park Street will integrate the market into the nearby retail environment. It will also enable the market to be seen by pedestrians entering the town centre from Bridge Street, upper Park Street and Digbeth, as shown in the visuals overleaf.

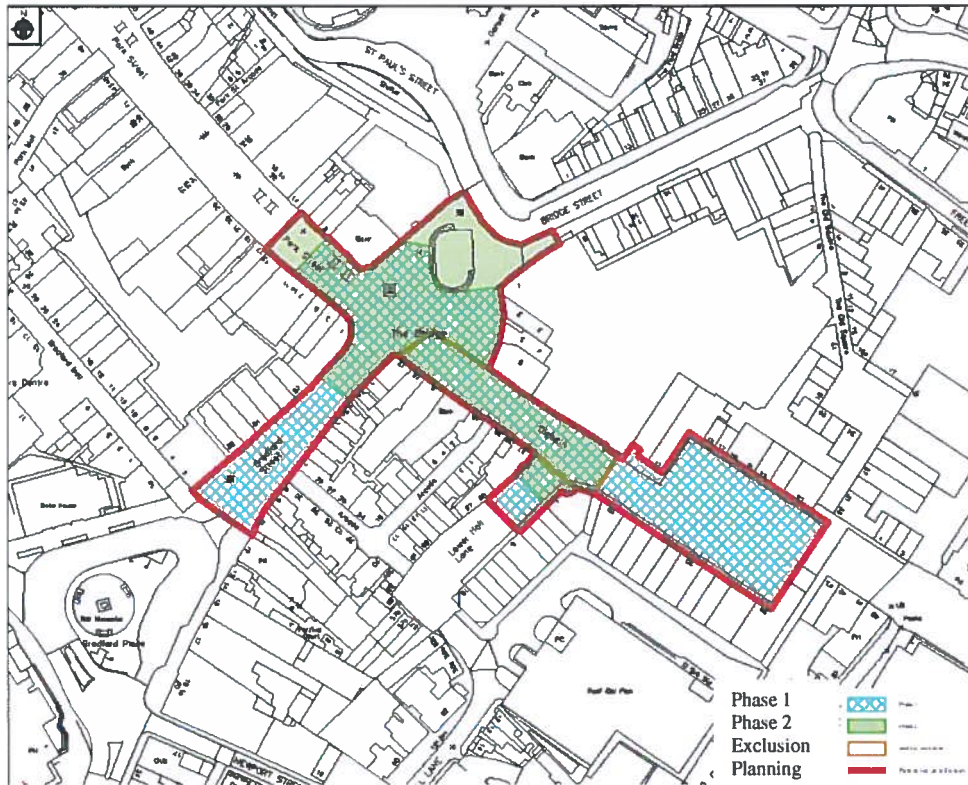


Figure 2 - Proposed market phasing plan. Refer to drawing ARP-PL-003 for individual phases.

2 Assessment Context

The key aims of the DAS are to:

Demonstrate the steps taken to appraise the context of the development and how the design of the development takes that context into account in relation to its proposed use.” (TCPO 2010, Part 2, Reg.8 (3b)). It is a requirement that context is discussed in relation to the development as a whole as well as individual elements e.g. layout (SI 2010/567).

This section may include reference to desktop research, on site observations and consultation feedback.

2.1 Physical considerations

The appearance of the town centre in regard to existing architecture, landscape features and access routes is summarised in the following key points.

- i. **Buildings and Facades:** Many shop frontages have been altered to accommodate trader’s requirements. Above ground floor level the façades of many of the buildings reveal the rich past of Walsall that has been lost at street level. Key heritage buildings include the late 19th century Lloyds Bank and the Victorian Arcade, both providing distinct character and quality. This theme is explored in more detail in the Heritage Statement that accompanies this submission.

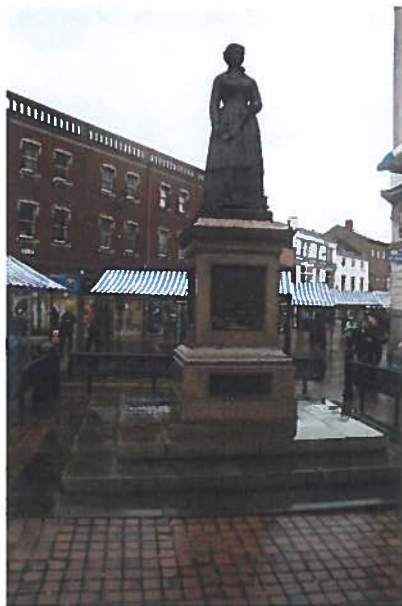


Figure 3 - Key heritage assets to be retained and made more focal: The Sister Dora Monument and the Hippo Sculpture.

- ii. Streetscape surfaces: There is a considerable mix of surface treatment within the study area. This varies from tarmac, through standard block paving, tumbled Tegula paving and limited higher quality stone. Many surfaces are damaged, with evidence of previous schemes being mixed in with repairs poorly carried out.
- iii. Street furniture: There are various types of street furniture within the study area. These include telephone boxes, street advertising units, litter bins, benches, moveable planters, street lights, bollards, signs and public art. Much of the furniture was implemented at different times and therefore gives an uncoordinated appearance creating a cluttered environment that is difficult to maintain.
- iv. Market: The market has fixed stalls. Traders use their space to maximum effect, although many stalls are often unused. The stalls are set out in a linear layout due to the current physical constraints of the area, creating a sometimes poor relationship between the stalls and the shops. The trade waste disposal bins are not clearly positioned, which together with difficulties in cleansing the street due to physical constraints, can sometimes mean that pedestrian walkways are partially blocked.
- v. Planting: A small amount of planting is evident. There are mature trees on Park Street and semi mature trees on The Bridge and side streets. There are also some trees in planters on Park Street. The choice of Tilia (common lime) tree has resulted in the dropping of a sticky substance which is hard to clean off the surrounding paving and market stall covers. Please also refer to the Tree Survey that accompanies this submission reference TC MF8217.
- vi. Existing drainage is limited and consists of surface channels which pose possible trip hazards where damaged, draining to a variety of differing gully and channel gratings. Some of these have failed or have been in-filled. Some localised ponding in heavy rainfall is common outside of the Lloyds Bank frontage.

2.2 Social considerations

The current market operates on five days of the week, being closed on Thursdays and Sundays only. Wednesday is acknowledged as the quietest day, with Saturday the peak trade day. Currently at its peak, there are 51 traders and approximately 70 stalls in use based on October 2014 data. It is proposed that the new market, on the completion of Phase Two, will have the potential to operate up to 7 days a week if so required in the future.

The market currently accounts for approximately 2% of the town centre's GDP and is recognised as a traditional and historical part of the town centre; its presence contributes to visitor numbers to the centre.

Phase One of the new scheme will continue to operate under the existing constraints, but will be rationalised under Phase Two.

The new public realm and market seeks to enhance the shopping experience, and the demountable stalls allows a flexible approach with the ability to provide a civic space that can be used by the community for events.

2.3 Economic considerations

Walsall Market is currently located in the area of Digbeth, lower Hall Lane, Bradford Street and The Bridge with temporary planning permission until April 2015. This planning permission is now required to enable Walsall Market to establish a permanent home in the proposed location, thereby replacing the current temporary consent on which the market is located. The temporary consent is due to expire in April 2015 (planning application reference 13/0433/FL).

Previous viability studies carried out by GVA indicate that the market has been in economic decline for several years and requires improvements to avoid further decline.² Whilst the market is still identified as an important part of the town's retail offer, current issues affecting the site include low quality products, a fragmented layout, poor way-finding, unappealing appearance and a lack of identity.

Also of importance are two major retail schemes recently consented within close proximity to the market:

- The St Matthews Quarter retail development (planning reference 13/1421/FL) will provide a new retail frontage along the southern side of High Street and the eastern side of Lower Hall Lane. The development will provide 11 units, mostly shops (A1) with some financial/professional services (A2) and food places (A3 and A5) with a total floor space of 8,257m².
- Old Square Shopping Centre (planning reference 12/1666/FL) will create a large retail presence, with flagship stores including a Co-operative food store fronting The Bridge and Primark fronting Digbeth.

The delivery of these major retail schemes, in addition to the temporary planning consent that is due to expire, compound the need for a comprehensive review of the long term location and design of the market. The applicant has therefore sought to revitalise the market with a new design, improved quality and central location, to create a commercially successful, well-managed market that offers:

- A choice of competitively priced goods which serve local needs or particular demand.
- Local employment – with many traders living locally;
- Opportunities for entrepreneurship - particularly from the ethnic communities – where new start-ups can flourish in a low cost/low overhead environment.
- A positive, safe and enjoyable shopping experience for residents and visitors to the town.
- Encourage social interaction for all sections of the community at the Bridge which makes a positive contribution to the vibrancy of the town.

It is hoped a rejuvenated market in a permanent, central location with enhanced urban realm will secure the long term future of the market.

² GVA and the Retail Group (2012) Walsall Market: Options Appraisal

2.4 Policy considerations

As required by Section 70(2) of the Town and Country Planning Act 1990 and Section 38(6) of the Planning and Compulsory Purchase Act 2004, the proposals must comply with the development plan. The development plan for Walsall consists of the saved policies within the Unitary Development Plan (2005) and the Black Country Core Strategy (2012). The National Planning Policy Framework (NPPF) is a material consideration in planning decisions.

It is considered that the proposed development accords with the objectives of national and local planning policy.

In particular, the proposals will ensure that the market remains attractive and competitive, in line with Para 23 of the NPPF. This will further support the viability of Walsall's town centre and primary retail area, as required by the NPPF, Policy S2 of the UDP and Policy CEN3 of the BCCS. The proposals also comply with the relevant national and local policies regarding high quality urban design, protection of heritage assets, accessibility for all, appropriate integration with the surrounding landscape and incorporation of consultation responses.

For a full assessment of the proposals against the relevant local and national planning policy, see the **Planning Statement**.

3 Community and Business Involvement

3.1 Consultation Strategy

In developing the design proposals for Phase Two of the Walsall Market project, the Council and Arup sought to consult key stakeholders and the local community and achieve an on-going and effective dialogue about the location, design and layout of the market.

A Consultation Strategy was prepared to ensure that a well-planned and inclusive approach to consultation took place. The pre-application consultation approach is considered best practice and proportionate to the scale and nature of the proposed development, for both phases One and Two. The consultation process included various publication techniques, a public exhibition, feedback forms and meetings with key stakeholders.

The consultation process referred to the final scheme, after the completion of Phase Two of the works. The Phase One works are a necessity to ensure the continued successful operation of the market throughout the duration of the works, as well as during the Network Rail Electrification Works and the implementation of the market Exclusion Zone and as such they are to be temporary arrangement only, before the progression to Phase Two. The need for Phase One is explained in more detail in the Planning Statement that accompanies this submission. The design, layout and concept for Phase One builds on that of the existing market which has previously widely been widely consulted on with stakeholders.

The **Statement of Consultation** provides more detailed on the pre-planning application consultation undertaken, the feedback received, and the response of the project team. The following section of the DAS provides a summary only.

3.2 Consultees

The pre-planning application consultation process began in November 2013 and has included consultation with four groups:

- Technical and statutory stakeholders.
- Local retailers and business representatives.
- Market traders.
- The local community.

The consultation took various forms including questionnaires and leaflets, internet based information, invited presentations, and culminated in a public exhibition over two days on 21st and 22nd March 2014.



Figure 4 - Public Exhibition Display

3.3 Feedback

As summarised below, the feedback received during the pre-application consultation period was largely supportive of the proposals:

- 94% of respondents agreed that Walsall Market needs improving.
- 72% of respondents supported the location of the new market at the Bridge and lower Park Street.
- 73% of respondents supported the proposed layout.
- 83% of respondents agreed that there should be demountable stalls at the new Walsall Market.
- 70% of respondents rated the proposals as 'Very Good' or 'Good'.

4 Option Development and Evaluation

4.1 Constraints and Opportunities

In the development of the options and the Phase Two final proposal for the market and urban realm improvements, consideration was given to both the constraints and opportunities imposed by the situation. The key items are listed below.

The identification of the need for Phase One arose as a result of the consideration of these constraints and opportunities. The need for Phase One is explained more fully in Section 3 of the Planning Statement that accompanies this submission, with the constraints and opportunities that specifically relate to Phase One included below.

4.1.1 Constraints:

- Planning Exclusion Zone adjacent to Primark on Digbeth needing to be implemented by April 2015.
- Works associated with the reconstruction of the rail tunnel below Park Street as part of the Network Rail Electrification scheme to commence April 2015, requiring creation of space on The Bridge to allow for safe vehicular movements in the area.
- The Network Rail Electrification Works imposing restrictions on where stalls can be relocated/town centre works can take place for the duration of the rail works (expected to be until late 2015).
- Timescales for both of the above constraints sitting outside of realistic timescales for delivering the full, permanent market.
- Working with retailers adjacent to the market.
- Vehicle access routes for deliveries, maintenance and emergencies.
- The maintenance and upkeep of urban realm works.

4.1.2 Opportunities:

- Improved urban realm, image and public safety.
- Enhance the sustainability of the market, both commercially and environmentally.
- Link to the redevelopment sites.
- Consolidate the location of the market and create an efficient market.
- Enhance accessibility.
- Enhanced civic space available for community use.
- Improvement the market quality and offer.
- Facilitation of the Network Rail Electrification Works and the implementation of the Primark Exclusion Zone through the introduction of a phased approach to the market project.

4.2 Design Options

The development of design options can be categorised into two stages; location identification and market layout.

4.2.1 Location

Initial design development focused on identification of the preferred location for the new market within Walsall. This built on the baseline market analysis and urban realm assessment undertaken by GVA in 2012 to establish 12 potential market locations.

These were identified on a plan and formed part of the early consultation process. A traffic light system was applied to an assessment table in order to summarise the key findings and recommendations. This assessed each location against seven key requirements, providing a comment and indicative ‘score’ to each to offer an objective approach to the location selection.

Bradford Street, The Bridge, lower Park Street and the Saddlers Centre all scored highly, with the Jerome Retail Park proving most unsuitable.

Following further consideration of development timing and phasing, the area on The Bridge and lower Park Street were selected as the preferred location.

Potential Market Location	Complies with planning policy	Enhances public realm	Unconstrained by infrastructure	Positive impact on trading	Compatible with other uses	Is accessible to all	Affordable and low risk
A Bradford Street	●	●	●	●	●	●	●
B The Bridge and lower Park Street	●	●	●	●	●	●	●
C Digbeth	●	●	●	●	●	●	●
D Lower Hall Lane	●	●	●	●	●	●	●
E Upper Park Street	●	●	●	●	●	●	●
F Gallery Square	●	●	●	●	●	●	●
G Old Square Shopping Centre	●	●	●	●	●	●	●
H Victorian Arcade	●	●	●	●	●	●	●
I Jerome Retail Park	●	●	●	●	●	●	●
J Saddlers Centre	●	●	●	●	●	●	●
K Shannon's Mill	●	●	●	●	●	●	●
L Tesco / Crown Wharf	●	●	●	●	●	●	●

Figure 5 - Summary matrix of market location options

Phase One was considered against the same location options analysis. Building upon these principles and the fact that the existing market is performing well in its location on The Bridge and lower Park Street, Bradford Street and Market Square, Phase One will continue to utilise these locations for the siting of market stalls

until Phase Two is implemented. Maximising the space available for stalls on The Bridge whilst maintaining stalls in Bradford Street and Market Square in Phase One will contribute towards the overall aim of minimising disruption to traders, retailers and the public in both Phase Two and also the wider programme of regeneration taking place in Walsall Town Centre.

Stalls will be removed from Bradford Street as part of Phase Two in order to achieve the preferred, consolidated layout on The Bridge (as discussed in more detail below). Future public realm improvements in Bradford Street would be possible under Permitted Development rights, but have been omitted from this application so as to focus on the market itself and its immediate vicinity.

4.2.2 Layout

With the location identified for the final phase two permanent scheme, the layout developed takes account of existing best practice for the use of modern, demountable style stalls, instead of the existing fixed stalls. The overall concept was to provide a central focus to the market, with themed stalls radiating from this, including food and drink provision, entertainment areas and relocating the public art. This provides a coherent and cohesive setting for the market, while also working with the existing and future setting. Inherent to the stall concept is to set them out with an 'active edge' for the customer interface.

Four outline design options were prepared for the public consultation event. Each had a clear set of strengths and weaknesses that are provided in the Urban Realm Strategy. All of the options sought to ensure that the Market becomes a destination. They all ensured that key views remain clear from Park Street up to Debenhams. All options sought to address the concern relating to market stalls obscuring shop frontages.

The preferred option was presented at the Public Exhibition and subsequently amended to take into account comments received from that event.



Figure 6 - Market layout presented at public exhibition prior to amendments.

5 Design

The proposed design for Phase One will include the positioning of up to 76 stalls and 4 catering vehicles in lower Park Street, The Bridge, Bradford Street and Market Square, until Phase Two is implemented. Phase One also includes the removal of 6 trees in south-eastern area of The Bridge and the relocation of the Hippo sculpture to the Civic Quarter, both to be reinstated on the Bridge in Phase Two. The Phase One layout is shown below.

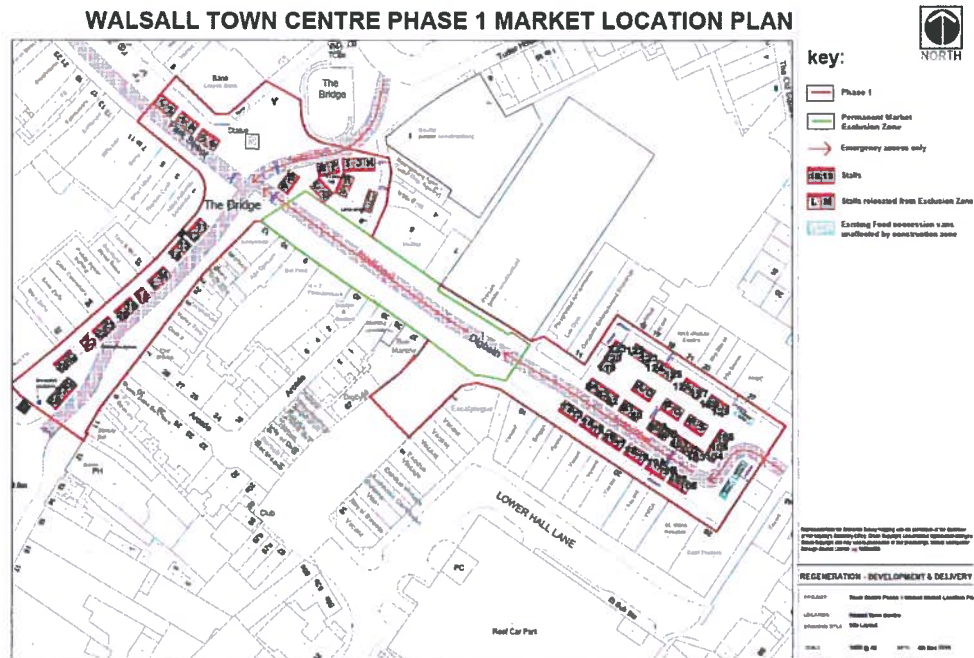


Figure 7: Phase One layout

The proposal for the final Phase Two scheme will provide space for up to 70 stalls. These will be set on an improved layout, consist of the new demountable stalls and provide flexibility in terms of both display and product mix. The stalls will consist of a flexible mixture of 3x3m stalls or 3x4.5m stalls that can in places be interchanged to suit daily market demand. Up to 16 larger stalls can be provided for within the market layout, reducing overall stall numbers to a maximum of 62.

With paving materials, lighting and the selection of street furniture has been undertaken to bring together a series of elements that respond to the improvements and setting of the centre of Walsall Town. It is intended that an attractive open space is provided when the stalls are not present.

It is intended that all elements will be robust enough to ensure limited regular maintenance and a long operational life that in turn provides the necessary uplift to the market and shopping district.

5.1 Use

- Total number of stalls in Phase Two up to 70 with possible future expansion available along Bradford Street as part of a later planning application, if necessary.
- Flexibility to permit up to 16 larger 3x4.5m stalls within the Phase Two scheme.
- On completion of Phase Two, offers the ability to have a large open public square for occasion town events.
- Improves access along Bradford Street for HGV deliveries to retailers.
- Provides good linkages to Lower Hall Lane.
- HGV, Emergency and refuse vehicle access possible in all directions.
- Access to retail frontages not compromised.
- Phase Two Places Sister Dora at the heart of the proposed refurbishments as a key way finding point.

5.2 Layout

5.2.1 Stall Type

The stalls proposed in Phase One are to be relocated existing stalls; new stalls will be procured as part of the Phase Two works and are explained in more detail in this document.

The Phase One market stalls will be as existing, with the layout comprising of stalls relocated out of the Exclusion Zone (as detailed in Section 3). The current stalls are not demountable; they remain erected at all times although can be moved and modified for alternative locations by specialist fabricators if there is a specific need to do so. The active area of the stalls measure approximately 3.0 x 2.5m, in plan form with an additional frontal overhang, and have a 2.1m eaves height and 2.9m roof crest height.

The existing stalls consist of a four-legged metal frame with variable table options depending on trader demand. The stalls have blue and white vinyl roof canopies that can be removed for repair or replacement as necessary. An example of the existing stalls can be seen below:



Figure 8: Typical existing unoccupied stall

The market stalls in Phase Two will as standard consist of:

- Structural four legged frame
- Marque roof
- Rear panel
- Ground fixing
- A table front
- A hanging frame

These stalls will be demountable, and could be removed for public events or if a smaller market is required. However, the positioning of the stalls will be fixed with permanent infrastructure in place including ground sockets. The style of this socket will be dependent on the market stall model ultimately purchased. Regardless of the product chosen, the sockets will be flush fitting with no tripping hazard. To permit the flexibility of using two stall sizes, 32 additional sockets will be required, the use of which will be dictated by the stall arrangement adopted on each market day. Drawing ARP-PL-025 shows the arrangement of the market without any stalls being present, with the floor fixing locations evident.

The stalls will be 3x3m or 4.5x3m in plan size, with approximately 2.1m eaves height and up to 3.6m roof crest height. They will consist of a modular four

legged aluminium frame. These would be covered with a hard wearing fire retardant UV protected PVC or polyester material coloured to Walsall Council's specification of Navy Blue (RAL 5003 equivalent) and Light Grey (RAL 7001 equivalent). The sides of the stalls may be individually removed to suit layout requirements and may be transparent to provide a visually open market, whilst offering weather protection. This is more clearly illustrated below.



Figure 9 – Rendered image of proposed stall appearance. The internal arrangement may differ to suite traders specific requirements.

The outdoor seating area will use up to eight proprietary stainless steel or aluminium folding leg tables, with umbrella type sun shade canopies. These will be accompanied by four stainless steel stackable chairs per table. There will not be permanent ground fixings for these tables and they will be arranged daily by the market management team and removed to storage each evening. As with the current market, it is envisaged that the local food traders will self-regulate the layout of the tables throughout the day.

5.2.2 Stall Arrangement

In Phase One, the stalls will continue to be arranged in a largely linear layout due to existing constraints within the town centre such as loading capacities, maintenance of vehicular accesses and location of street furniture. Phase Two offers the opportunity to rationalise these constraints and arrange the market stalls in a back-to-back layout, as explained below.

As part of Phase Two, the stalls are to be arranged with a 'back to back' layout, so that the active frontage of the market stalls is facing the pedestrian routes and adjacent shop/business facades. Traders and storage areas back onto one another, with access provided from the frontage or stall sides depending on position. This will connect the market stalls with the adjacent retail and business frontages.

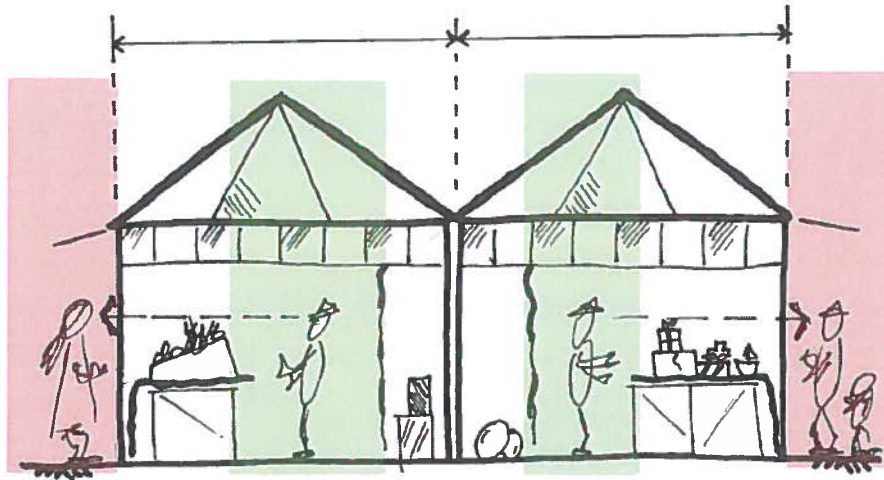


Figure 10 - Back to back philosophy

In Phase One, as existing, stalls will be positioned no closer than 3m from buildings in order to maintain good pedestrian and disability access and flow, not only to the market itself but also to the surrounding Town Centre. Stalls are to be arranged in no more than double or treble formations to allow pedestrian, trader and disability access. To ensure this remains so, the encroachment policy for the market is being reviewed. This will ensure that the way in which traders display their goods within the footprint of their stall will prevent obstructions, aiding pedestrian flow around the stalls, especially in The Bridge area.

The paving materials and street furniture as part of Phase One will remain the same as the existing, therefore are not expected to present any access issues in themselves.

For Phase Two, pedestrian routes between the stalls are at minimum 3m wide. The market layout provides an ordered and well defined market space with spacious walkways. The layout provides a high number of double corner stall facades, helping to maximise sale areas.

The stall arrangement in Phase Two has been notionally split into two zones. A food zone, consisting of both hot and cold food (both groceries and consumables) is located on the eastern edge of the markets, whilst none food related stalls occupy the remainder of the market space. The food zone stall groups have access to a higher rated 32A power source, but are otherwise no different, and it is feasible to mix up these zones if required in future market operations. This zoning arrangement has been developed to ensure that the food stalls are located:

- Together to encourage competition and therefore quality.
- Adjacent to the main access route, to enable maximum flexibility for the use of catering vehicles.
- Adjacent to the main access route, to give maximum aisle space for any queues of customers that may arise at peak times without blocking the market.

- Adjacent to the main seating area in the triangle, to best serve this focal café style area.
- Away from other stalls where the smells arising from food preparation could interfere with others.

The Bridge market area is clearly bound with different paving styles, lighting, bollards and other street furniture elements. The north eastern boundary of The Bridge market area is to be bound with bollards of approximately 900mm tall and be BSI PAS 68 rated, for cars travelling at 30mph, to protect against accidental or malicious vehicle impact from Bridge Street. In addition, refuse collection facilities will be provided, including recycling bins at the north east and south west boundaries. The north-west and south-east boundaries are defined with flush kerbing to define the market area whilst allowing free flow pedestrian movements to the wider areas. Areas of vehicular access are to be distinguished with contrasting pavement types.

Consolidating the market stalls within The Bridge, a key civic space, with effective boundary treatment achieves a vibrant market place. The stalls on lower Park Street integrate the market into the nearby retail environment. It would also enable the market to be seen by pedestrians entering the town centre from Bridge Street, upper Park Street and Digbeth.

5.2.3 Stall Storage

The demountable stalls retract into stackable units. This will be transported to and from site on a dedicated flatbed trailer to a council owned storage depot facility in Pelsall when not in use. Further details on this can be found in the Operational Plan that accompanies this submission.

5.3 Principal Market Stall Location

Following the identification of The Bridge and Park Street as the preferred option, the new Market design on completion of Phase Two, has focussed on ensuring the location will be able to operate as both a market place and public open space when stalls are removed. It was important to ensure emergency vehicles, traders vans and HGVs delivering to retail outlets on Park Street during the evening could continue to use Bradford Street and these requirements have been addressed in the public realm improvements to the street being suitable. The Bridge has become inviting to shoppers with a less cluttered location with rationalised tree planting, muted paving, seating and art pieces which will begin to be implemented as part of Phase One. Shoppers arriving to Walsall by bus can be dropped at the main bus station to the north east and proceed directly to the market along Bridge Street.

5.3.1 Bradford Street

Following consultation and discussions with Walsall Council it was agreed that the final phase two preferred option would omit market stalls along Bradford Street, rationalising the public realm within the streetscape. This will also contribute towards achieving the preferred, consolidated layout on The Bridge and lower Park Street. During the Phase One works, the existing established stalls on Bradford Street will remain (including the relocation of up to two stalls into Bradford Street) in order to contribute towards the overall aim of minimising

disruption to traders, retailers and the public in both Phase Two and also the wider programme of regeneration taking place in Walsall Town Centre.

It is important to ensure that emergency vehicles, traders vans and HGVs delivering to retail outlets on Park Street during the evening can continue to use Bradford Street, both during phase One and Two. This has been achieved by maintaining a minimum 4m wide clear path through the area.

On completion of Phase Two, the street will be more inviting to shoppers as it will no longer contain permanent market stalls.

Future public realm improvements to Bradford Street could be carried out under permitted development rights outside of this application.

5.3.2 Digbeth & Primark Site

Digbeth, towards the proposed Primark site has, as part of the wider Exclusion Zone, had all existing market stalls removed in Phase One to be maintained in Phase Two, and has also had the public realm within the streetscape rationalised. It was important to keep this connection clear for emergency vehicles and traders vans. It is expected that footfall will be increased significantly within this part of the town centre upon the opening of the new Primark store.

5.4 Amount and Scale

The number of stalls in Phase One has been determined following analysis of the current stall and occupancy numbers, mindful of the proposed timescale for the implementation of Phase One in March/April 2015. The works to be implemented as part of Phase One consists of the start of the overall public realm works with removal of the existing trees in The Bridge area (to be replaced in Phase Two) to facilitate the relocation of eleven stalls and one catering vehicle on The Bridge. These relocations will also require the existing Hippo sculpture to be temporarily relocated to the Civic Quarter, to be reinstated in Phase Two.

In addition to the stall relocations in The Bridge area, it is proposed to relocate up to six stalls into Market Square and two stalls into Bradford Street in addition to the existing stalls in these locations. These relocations are necessary in order to implement the Exclusion Zone in Digbeth and to facilitate the Network Rail Electrification Works taking place in the Town Centre, in 2015. Both of these factors are explained in more detail in the Planning Statement that accompanies this submission. Therefore, in total, Phase One proposes up to eleven market stalls and one catering vehicle at The Bridge and 17 stalls at Bradford Street, six stalls at lower Park Street and up to 42 stalls and three catering vehicles in Market Square for a temporary period of time, prior to the implementation of Phase Two.

In Phase Two, the maximum number of stalls is 70 with specific additional provision for two catering vehicles in the triangle area. Each stall is 3m x 3m or 4.5m x 3m square, providing a total trading space of 630m². Up to 16 larger stalls can be installed, which reduces the overall maximum stall number to 62 plus specific provision for two catering vehicles in the triangle area. The public realm improvements are proposed across the remainder of the study area within the phase two planning application boundary. In both arrangements, it is possible to replace any two adjacent stalls with further catering vehicles if so desired.

The number of stalls has been determined following analysis of the current stall and occupancy numbers. Whilst the current market has just over 100 stalls, the market only consists of approximately 50 traders on peak days, with up to 70 stalls in use as per October 2014 occupancy figures. As it is intended to increase the quality and offer of the market in its new configuration, it is envisaged that there will be some natural loss of some existing traders. This stall quantity therefore provides sufficient space to accommodate the current market trader's demands, whilst encouraging competition for spare spaces to ultimately increase the overall standard of the market offer.

These proposals have been carefully developed throughout the study commission, including the public consultation event, traders association input and various comments from the relevant council officers. It is considered that the proposed amount and scale will achieve the key consideration of commercial and environmental sustainability.



Figure 11 - Proposed Phase Two market layout after consultation - maximum stall number using 3x3 stalls. Refer to drawing ARP-PL-020.



Figure 12 - Proposed Phase Two market layout after consultation - maximum stall number using mixture of 3x3m and 3x4.5m stalls. Refer to drawing ARP-PL-021.

5.5 Landscaping Strategy

Initial thoughts for the permanent market as part of Phase Two are muted elements that are of the appropriate scale, texture and colour. It is intended that materials will define spatial activities when the market isn't in operation. The selection process must ensure materials can be repaired or sourced easily without the need to have significant items stored away from site as this has historically proven problematic for Walsall Council.

5.5.1 Street furniture and paving

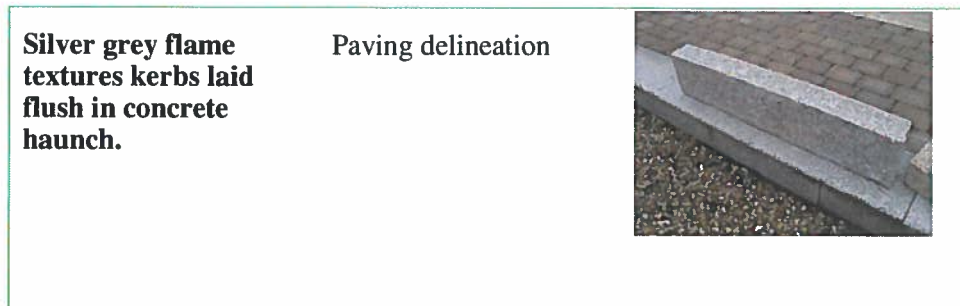
Street furniture and paving will be proprietary products chosen from mainstream supplier catalogues.

Benches must have backs and arms to provide support for the infirm or disabled, and also reduce their appeal to skateboarders and other anti-social behaviour. Wooden slats should be avoided due to their ease of breakage or disfigurement by vandalism.

Paving will be laid onto existing construction with only the paving and immediate bedding layer being replaced as part of the scheme. Paving will be laid onto a high performance hydraulically bonding mortar, such as SteinTec tuffbed, to provide a shrinkage free stiff bedding for the paving units. It is proposed that a free flowing slurry grout such as StienTec tufftop be used to penetrate the joints and cavities in the paving to finish the paving appearance.

Several paving types will be used as described below:

Surface Finish	Location for use	Image (Example only)
Existing Natural Stone reused from The Bridge area	Café style eating area	
Silver grey steel reinforced concrete flag units 600x600x63mm	General footways	
Mid grey clay brindle block pavers 200x100x50mm laid in herringbone pattern.	Vehicular areas	
Black fleck, rough-hewn textured blocks 300x200x60mm.	Building facades	



Bollards will be used throughout the scheme to establish pedestrian routes and also restrict unauthorised vehicle access. Bollards will be 900mm tall with a reflective strip. Bollard spacing will be at a maximum of 1.5m centre to centre, to restrict public vehicle access. Removable bollards will be lockable with a common master key and consist of a flush fitting hinged cap that falls into place when the bollard is removed. Bollards along the frontage of Bridge Street will comply with BSI PAS 68 rating, for cars impact up to 30mph, to protect against accidental or malicious vehicle encroachment from Bridge Street.

5.5.2 Waste disposal

Waste disposal and refuse collection is a key part of the new market development. The market currently operates with ten 1,100 litre Eurobins and eight 240 litre wheelie bins. These bins are randomly distributed around the market area, creating a cluttered and untidy environment.

In the new market scheme as part of Phase Two, the bins will be contained in two dedicated areas, each containing four 1,100 bins. This meets Walsall Council's aspirations to reduce refuse collection in coming years. The four bins in each location will be colour coded to allow a split of refuse type, to allow Walsall Council to achieve their aspiration of increased recycled content.

The bin locations in Phase Two have been selected to ensure that refuse collection vehicles can easily reach them for collection. The locations will be distinguished by a surrounding on three sides of bollards to match those adjacent with infill steel panels. These panels will be laser cut with the Walsall Markets logo, subject to appropriate advertising consent. The bins will be stored off site as per the current arrangement.

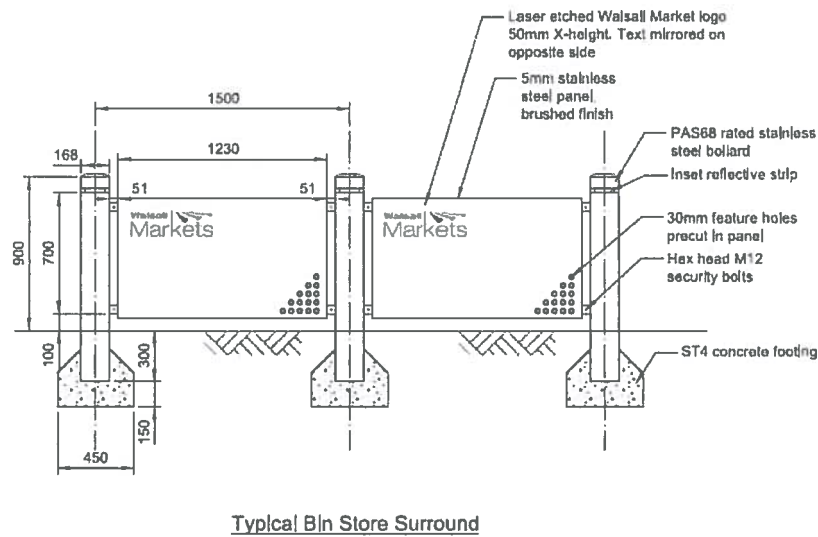


Figure 13 – Bin surround design proposal

5.5.3 Lighting

Lighting provision in Phase Two will take two forms. Existing lighting columns which provide the necessary levels of illumination will remain, or be only slightly repositioned to suit the new layout. All columns and lanterns will be replaced with new columns and lanterns.

Additional feature lighting columns will be added to the scheme to provide more specific illumination of key architecture or public art, and to highlight routes within the Market and public realm.

All lighting products will be by Philips or Thorn as approved by AMEY the PFI contractor.

5.5.4 Tree planting

New trees are proposed within the application area, to replace the existing trees removed. A tree survey has been undertaken for these existing trees and can be found in the planning documents. Six trees on The Bridge are proposed to be removed as part of Phase One, to be replaced in Phase Two. The existing Tilia (common lime) trees are unsuitable for the location due to the sap and fruit produced, which damages street surfaces and market stalls.

It is intended that new trees be fastigate creating a narrow tree with a clear stem, to avoid disruption to pedestrians. The exact species will be determined at detailed design, but preferred options are:

Pyrus calleryana

Hardy and drought tolerant, with deep roots to avoid surface disturbance.



Tilia cordata

Ideal for natural planting, with low sap content.



Betula

New species with strong white bark.



Carpinus betulus

Very upright tree ideal for avenue settings.



Planting containers are to be used in areas where trees can either not be directly ground installed due to below ground constraints, or they need to have the ability

to be repositioned for access in special circumstances. The containers chosen must have enclosed tops to prevent collection of litter.

In ground trees will not use tree grills as these have maintenance and litter collection issues. A specific surface treatment such as compacted sand and gravel, or resin bonded gravel should be used at the base.

All in ground trees will have tree protection frames at their base, to prevent damage from passing dogs and vandals.

Root barriers will be used for all in ground tree pits, to minimise potential damage to adjacent utilities and paving.

Further detail on the trees within the application area can be found in the Tree Survey that accompanies this submission.

5.5.5 Power Supply

Existing bollard type power supplies will remain as part of Phase One but will be removed and replaced with below ground power provision in Phase Two, to ensure that when the stalls are removed an open uncluttered space is available. The existing power supply below ground infrastructure will be maintained as far as possible.

The exact product will be determined at detailed design, but it will be a simple covered recess with appropriately rated cables as has been used elsewhere within Walsall town centre, such as NAL or Charles Endirect. Automatic hydraulically lifting units will not be permitted due to long term maintenance concerns.

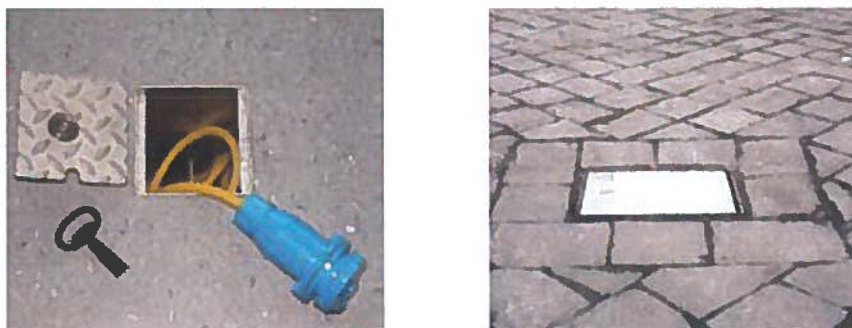


Figure 14 – Examples of GIFAS Piccolino 200 and Retrانت below ground power supplies.

A power point will be provided at each cluster of stalls, from which multi plug extension leads will be taken at high level within each stall.

Food-zoned stalls will have access to 32A power provision, and general stalls will have access to 16A power provision. A dedicated three phase power supply will be provided in The Bridge area for minor event power provision. It is envisaged that larger events will bring portable generators to the site.

5.5.6 Water Supply

Water supply will be provided at two points within The Bridge area, to give Traders access to fresh cold water. These supplies will be in the form of a lockable below ground stop tap, to prevent abuse out of hours. The supply will not be metered and water usage will be controlled by the management plan.

5.5.7 Drainage provision

The public realm surfacing will be drained using the existing underground sewerage infrastructure, and surface connections. It is intended that the surface levels remain unchanged from the current arrangement, other than necessary to improve falls for drainage purposes.

As part of Phase Two, surface drainage will be in the form of slot drains, using existing gully, or channel drain connection points. Slot drains provide an element of inbuilt storage provision, which benefits the wider drainage infrastructure by reducing the peak discharge value. Slot drains also provide a large area of drainage provision, without posing significant visual; impact or risk of surface collapse as can be the case with channel drains and gullies.

A separate dedicated point of drainage connecting to the combined sewer passing below The Bridge area, will be provided for Traders in the vicinity of the bin store and water supply to create a focal point for amenities. It will be necessary to control the use of this to prevent traders trying to flush large objects, food solids or fat into the system. This can be done through the use of a gully system with removable mesh and trap, to allow easy clearing out of such debris as part of regular maintenance regime. A formal S106 approval for connection will need to be obtained prior to final construction.

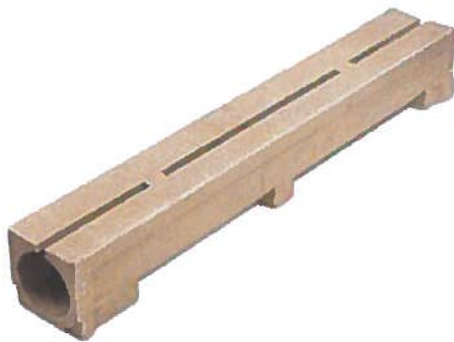


Figure 15 – Slot drain examples: ACO Technologies Plc Slimline and Q-max installation examples. www.aco.co.uk

5.6 Appearance

The intention is for the final scheme to provide the appropriate back drop for the public art elements and impressive buildings within the market vicinity. Along with this, the careful selection of high quality seating elements, litter bins, lighting columns and bollards will further accent and enliven the space. Additionally the selection of market stalls for Phase Two will work sympathetically with the urban realm improvements and ensure the continuation of a clutter free environment.

Refer to appendix A for indicative artists impressions of the finals scheme.

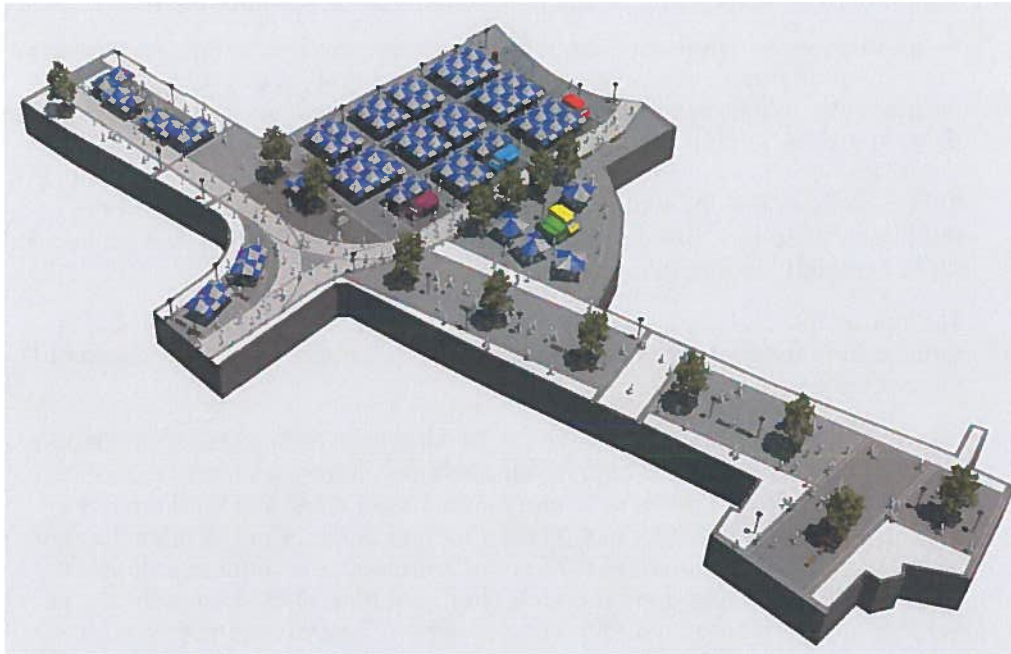


Figure 16 - Indicative aerial 3D visualisations of the proposed market. Image is an artist's impression to give an idea of scale and space. The detailed plans ARP-PL-020 and 021 should be referred to for further information. Also refer to Appendix A for further images.

6 Access

Access arrangements for Phase One of the market project will be in line with planned alterations to be made to the Traffic Regulation Order as required for the Network Rail Electrification Work. All other TROs remain unchanged from the arrangements for the existing market as part of Phase One. The layout of stalls in Phase One is designed to be accessible by emergency, delivery and maintenance vehicles. The terms of the current Traffic Regulation Orders will remain in place, with a 4m (minimum) wide emergency access being maintained off Bridge Street and across The Bridge, down Bradford Street, Digbeth and Park Street.

As existing, stalls will be positioned no closer than 3m from buildings in order to maintain good pedestrian and disability access and flow, not only to the market itself but also to the surrounding Town Centre. Stalls are arranged in no more than double or treble formations to allow pedestrian, trader and disability access. To ensure this remains so, the encroachment policy for the market is being reviewed. This will ensure that the way in which traders display their goods within the footprint of their stall will prevent obstructions, aiding pedestrian flow around the stalls, especially in The Bridge area.

The paving materials and street furniture as part of Phase One will remain the same as the existing, therefore are not expected to present any access issues in themselves.

The permanent market as part of Phase Two has been designed to be accessible by emergency, delivery and maintenance vehicles. It has considered the existing requirements of the retailers including the exit route along Bradford Street for large articulated vehicles serving retailers on Park Street. Consideration has also been given to existing areas where there is inadequate structural strength to support vehicles, and access by vehicles has been prevented through the use of bollards and street furniture. The access for Phase Two has been designed to adhere to the current intentions of the Traffic Regulation Orders, consisting of public vehicle restrictions between 10am and 5pm Monday to Saturday throughout the area, and a one way operation down Park Street from Gallery Square and Bradford Street to Bradford Place. Revisions to the TROs will be required to permit one way exit via The Bridge onto Bridge Street.

Delivery vehicles for market traders and retailers would be able to enter the site, as existing. This is via upper Park Street, or Lower Hall Lane, all exiting via Bradford Street towards Bradford Place or via The Bridge to Bridge Street. The application proposes that these existing routes will be clearly defined by contrasting paving styles. Adequate space would be provided within the stall layout for market traders to park temporarily to unload their produce before removing their vehicles to permitted parking areas.

The public realm for the market area consists of a level paved surface without any up-stands or kerbing. The paving will be laid to tolerances based on the Highway Agency Specification for Highway Works Series 1100, to minimise tripping and slipping hazards.

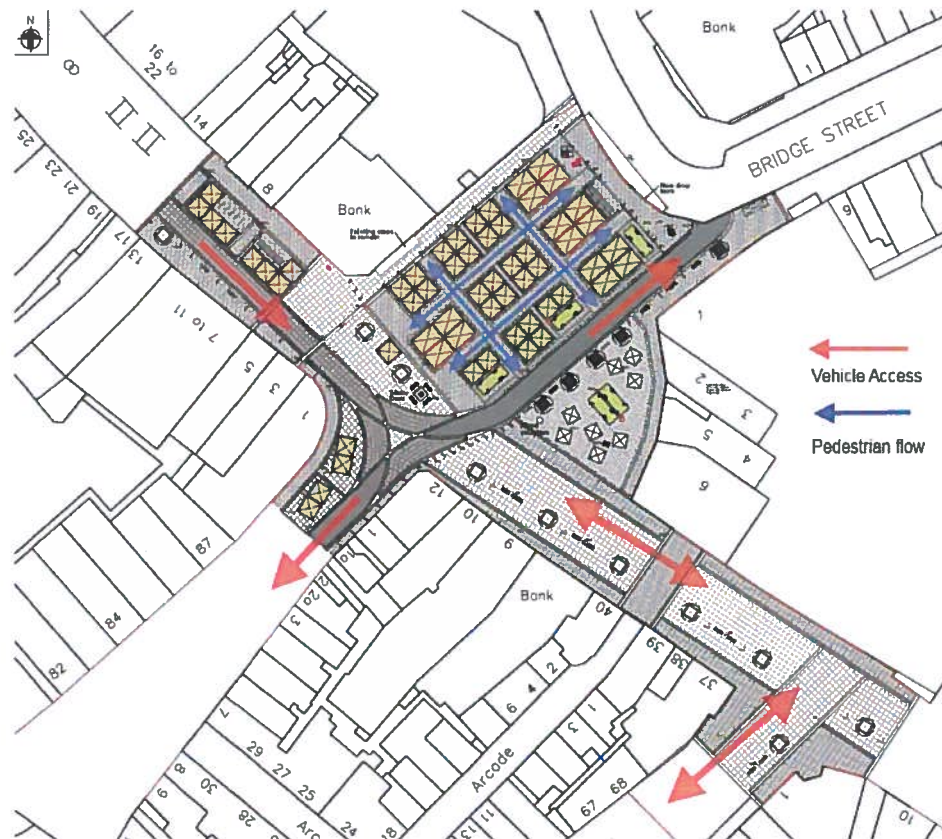


Figure 17 – Market stall indivisibility and access

6.1.1 Widths

It is generally regarded that a clear width of 2.0m allows two wheelchairs to pass one another comfortably, and accepted as the minimum under normal circumstances. The market stalls in Phase Two have been laid out with a minimum 3.0m aisle between stalls, to provide space for pedestrian movement and exceed minimum disability requirements. The widths between stalls in Phase One remains largely unaltered from the existing, on-the-ground layout that maintains these minimum widths.

As part of Phase Two, to the immediate east and west of the main stalls area, wide 5m plus corridors have been provided to direct pedestrian flows around the market stalls, and maintain a pedestrian flow equivalent to the current arrangement in which the podium constrains movement. Provision has been made to the east of the stalls to allow vehicles to access the stalls with space to pass stationary vehicles.

Strict enforcement of the new management strategy for the market is essential to ensure this minimum is maintained and stall goods do not encroach onto the walkways.

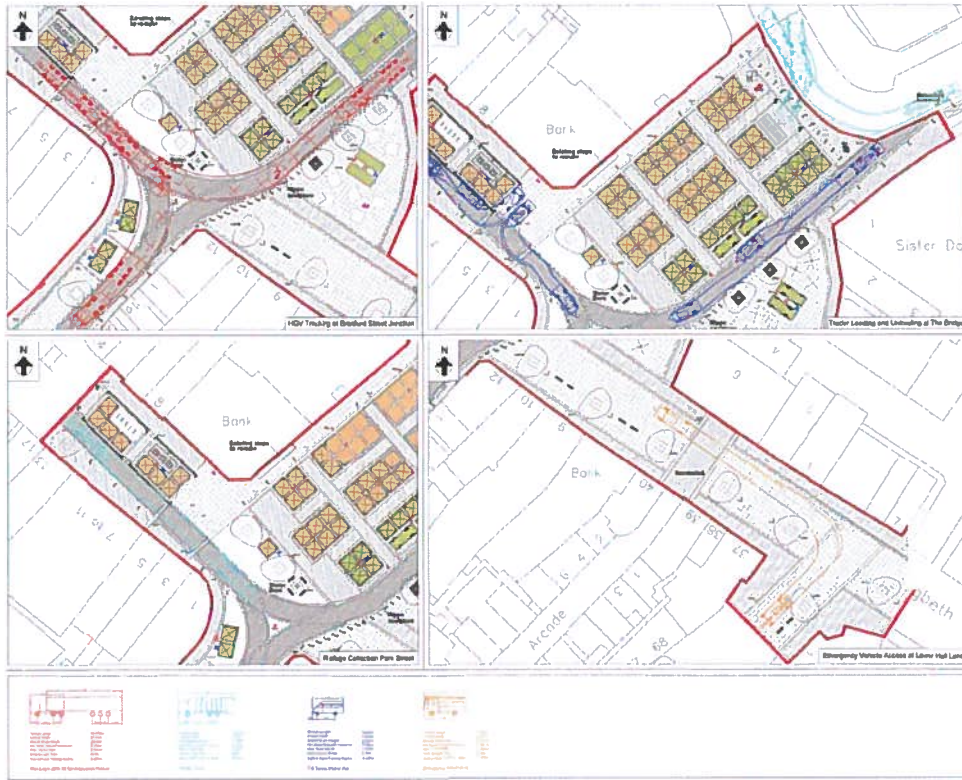


Figure 19 - Selected vehicle tracking manoeuvres

7 Conclusions

It is considered that the proposals accord with both national and local planning policies and are of benefit to Walsall Town Centre.

The proposals for both Phases assist in enhancing the market and retail offer and complementing the surrounding environment, while creating additional employment together with providing benefits to the local economy. The two phased approach for undertaking the scheme seeks to minimise the impact on the existing market traders by limiting their relocation and separation around the town centre.

In light of the above, it is respectfully requested that the proposals be recommended for approval at the earliest opportunity.

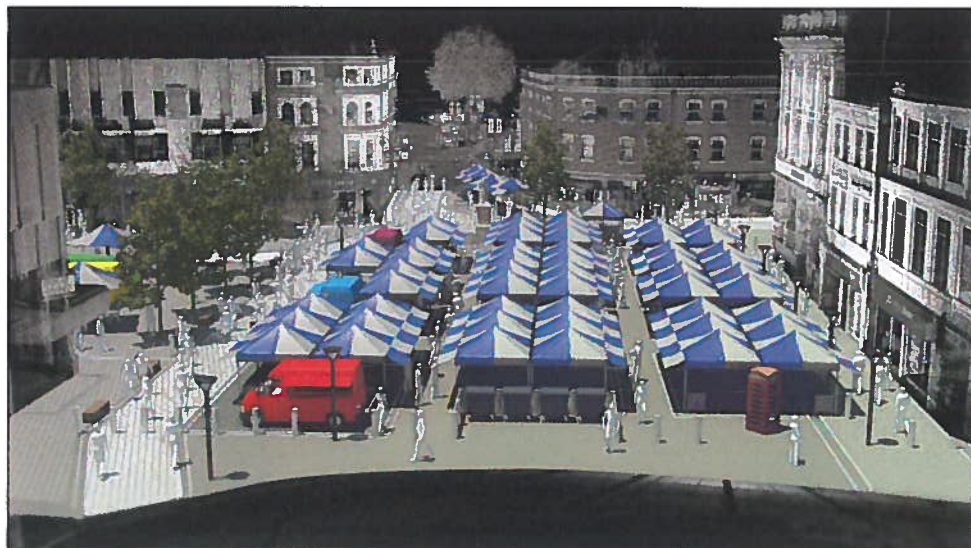
Appendix A

Artists Impressions of Final Scheme

A1



A – View of Sister Dora Monument from the corner of Digbeth and The Bridge.



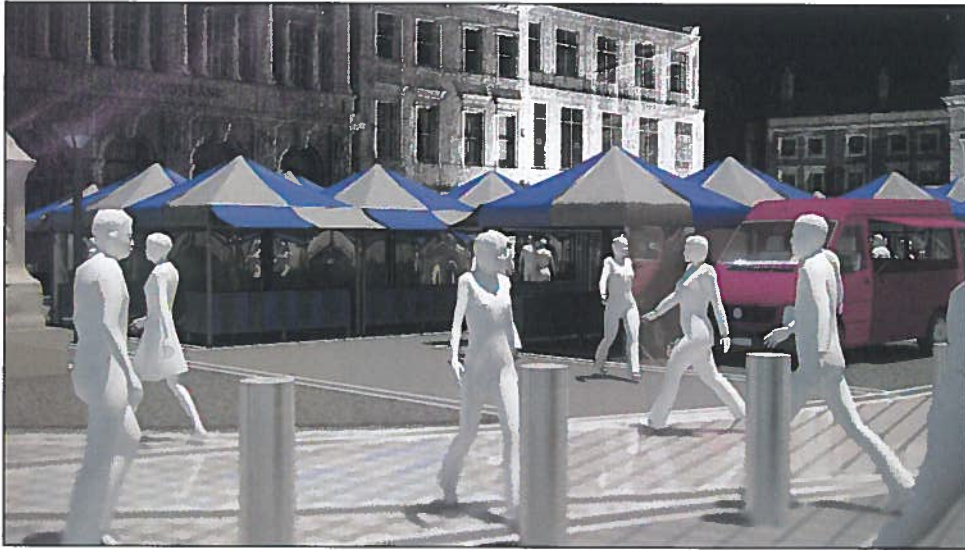
B – Aerial view of Market from Bridge Street end.



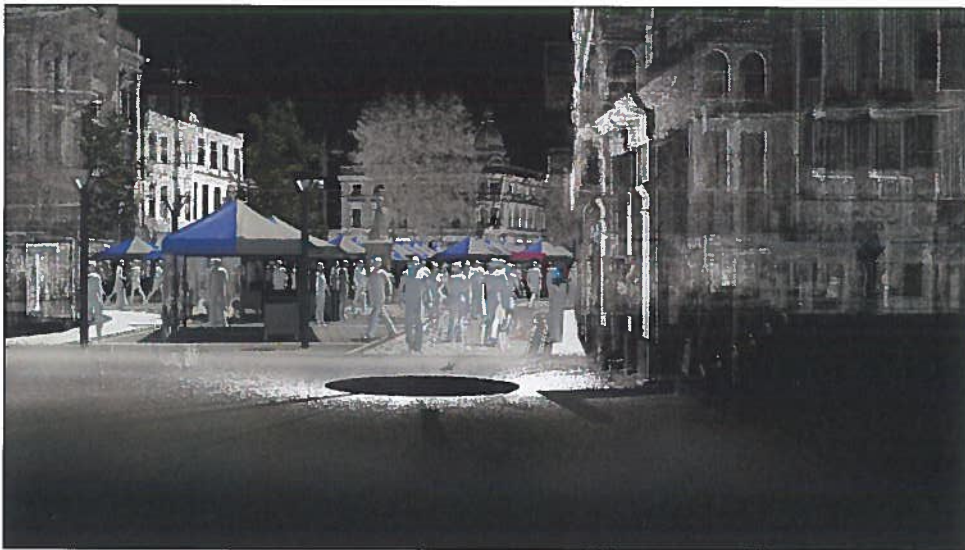
C – View along Park Street towards The Bridge and Digbeth.



D – View of Market from outdoor seating area.



E – View of Market entering from Digbeth area.



F – View towards market from along Bradford Street.