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Project Ref: Walsall Town Centre AAP
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Date: 25.04.17
Title: Bradford Place Background Note

Filenote

Walsall Town Centre AAP – Bradford Place Background Note

1. Introduction

Walsall town centre is well served by public transport with a rail station and two bus facilities all in the heart of the town. Walsall's main bus station, St Paul's was built in 2000 and is owned and managed by Transport for West Midlands.

Bradford Place is located at the southern end of the town centre of Walsall and acts as a strategic bus interchange facility within 5 minutes walking distance of St Paul's bus station, the railway station and the main shopping centre for Walsall. It has operated in this capacity for more than 80 years. The centre piece of Bradford Place is the Walsall War Memorial that dates from 1922 and was refurbished in 2004. The main purpose of the Bradford Place facility is for it to serve as a public transport interchange point for Walsall.

Bradford Place currently operates at a poor standard as a bus interchange facility. In keeping with the regeneration of Walsall town centre, it has long been an aspiration of Walsall Council's to improve Bradford Place bus interchange, design work was undertaken in 2004, with several potential options discounted and others taken forward with preliminary design work completed.

2. 2007 Option Appraisal

In 2007 an assessment of potential sites was undertaken. This included both long term options and short term & supplementary options. This work can be seen in full within appendices 1 and 2.

Of the long term options appraised the following scores were received.

1. Bradford Place 1 (Cenotaph relocated)

Redevelop Bradford Place interchange site and keep the current services operating from this location.

Score 80%

2. Bradford Place 2 (Cenotaph base unit structure realigned)

Redevelop Bradford Place interchange site and keep the current services operating from the site

Score 73.5%

3. Midland Road

Pedestrianise Bradford Place or create a public space in that area.

Score 38.5%

4. Soft touch option

Discontinue Bradford Place bus-interchange as it exists now. Pedestrianise Bradford Place to create a public space. Bus services currently using Bradford Place will be relocated and spread all over the town centre of Walsall.

Score 43%

5. Jerome Retail Park

Redevelop Bradford Place interchange site and keep the current services operating from area.

Score 76%

6. Lower Hall Lane Car Park

Relocate Bradford Place bus interchange to the disused Lower Hall Lane Roof Top Car Park.

Score 29.5%

7. Mountrath Street

Create a public space at around Bradford Place. Relocate services from Bradford Place to Mountrath Street.

Score 24%

8. Increase the capacity at St Paul's bus station with the 'Post Office' site

By increasing the capacity at St Paul's bus station, Bradford Place could be redeveloped with a public space.

Score 51%

9. Intown Row

Create a new 2 story bus station on the site at Intown Row.

Score 17.5%

10. New bus management system

This new bus management system would involve providing lay over space around the Caldmore area and Vicarage Road, away from Bradford Place.

Score 76.5%

Top 3 scoring options were

- 1. Bradford Place Cenotaph Relocation 80%**
- 2. New bus management system 76.5%**
- 3. Jerome Retail Park 76%**

The relocation of the cenotaph is not a feasible option and would have been difficult to politically pursue. Issues surrounding the cenotaph are discussed further in appendix 3.

The new bus management system stated would have consisted of the creation of layover space outside of the area. It would not have addressed the congestion and operational issues which have since increased within the facility.

Options for creating a new facility using land which is part of Jerome Retail Park began to then be explored over the next few years.

3. AAP Issues and Options Stage April 2013

Within chapter 8 of the AAP, entitled Transport, Movement and Accessibility the importance of public transport was discussed as being crucial for many residents, particularly given Walsall's population with its higher than average proportion of residents aged under 16 and over 65, who are most likely to rely on public transport. Public transport must strive to offer an attractive alternative to the car, as well as continuing to provide socially necessary services for the community.

The AAP offers the opportunity to consider if this provision is the best solution. Several options were explored and are shown below

Option 1

Re-develop St. Paul's into a larger more modern bus station

Walsall Town Centre Interchange

Having one large interchange would be preferable and given the continuing regeneration of the north side of the town, St. Paul's should be retained and expanded. It provides an accessible location for commuters, shoppers and for accessing further education at Walsall College. With this option, consideration could be given to the possible relocation of Walsall railway station to the location of Hatherton Road car park. However, given the way that the town has developed it may not be practical to achieve this unless some areas are de-pedestrianised to allow buses from the south and west to get across to the new facility. Relocating the rail station will also pose a significant logistical and financial challenge.

Option 2

A new multi-modal facility at Station Street

Station Street Interchange

This option would allow for buses currently using the Bradford Place Interchange to be re-routed to Station Street. This could provide the opportunity for Bradford Place to then be redeveloped as a public space with a memorial garden centred around the cenotaph. This option would greatly improve the interchange for bus, taxis and rail. It would also bring the facility closer to the middle of town, especially as much of the regeneration activity has taken place to the north of the centre in recent years. The presence of the low bridge at Bridgeman Street would be an issue and would require major support from the bus operators and Network Rail.

Option 3

Redevelop Bradford Place Interchange

This option would retain bus services in the Bradford Street area. However the existing facility is no longer adequate to cater for the volume of services that currently operate from this location, and additional provision could be achieved by expansion into Jerome Retail Park. Consideration must be given to retaining a transport interchange in the area if option 2 above is not feasible. Each of these options presents major issues in relation to funding and delivery. These would need to be explored in more detail to further understand individual issues surrounding each proposal.

Option 4

No major public transport changes

4. AAP Preferred Options Stage September 2015

The Issues and Options document included four options around improvements to bus services, three of which have been rejected:

Option 1

Re-develop St. Paul's into a larger more modern bus station

Walsall Town Centre Interchange

Rejected – creating one centralised bus station by redeveloping St Paul's would require buses to take a long detour from the south side of the town centre to a new enlarged facility. This would result in lengthened journey times making the bus less attractive to passengers. Alternatively, bus services would have to travel through The Bridge, which as discussed previously would not be a viable option. In addition to the issues relating to routing, in order to create a larger facility additional land would be required. St Paul's is currently constrained by land which is in use by businesses and services. These factors would make it difficult to expand the site and this option has not been included in the plan as it is unlikely to be delivered in the plan period.

Option 2

A new multi-modal facility at Station Street

Station Street Interchange

Rejected – creating a multi-modal facility at Station Street would allow passengers to easily interchange between different modes however this option does present several issues. Bus services travel into Bradford Place either via Bridgeman Street or Bradford Street. For those accessing Bradford Place via Bridgeman Street the rail bridge presents a height issue for double decker vehicles. Therefore this would limit the routing and fleet that bus operators could utilise. Changes to bus routing could result in detrimental effects to services and significantly impact upon journey times and reliability. Additionally, to accommodate bus services in this location land currently used as the rail station car park would be lost. These factors would make it difficult to deliver this scheme in the plan period. Further issues surrounding this option are discussed within appendices 4, 5 and 6.

Option 4

No major public transport changes

Rejected – providing no improvements to public transport interchanges within the town centre would impact in many different ways. To sustain and grow bus patronage, services need to be reliable. If no improvements were to be made to Bradford Place then it would become further congested hindering the operation of bus services. If, overall, public transport were to become less reliable, this could result in modal shift, or even reduction of trips and potentially impacting upon the economic growth of the town centre. Bus travel is an important mode of transport for residents in Walsall, therefore preventing growth and improvement would be detrimental to not only the economy of the town centre but also social aspects of daily life.

Preferred Option

The AAP Draft Plan has therefore progressed with Option 3 which looks to redevelop Bradford Place interchange to create larger capacity by expanding into Jerome Retail Park. This option is considered the most deliverable as the site is the least constrained and already has good access for buses. The amount of land needed from Jerome Retail Park will be fully determined as the scheme progresses, with the Council working with landowners to relocate businesses.

The Issues and Options document also included two options around improvements to Walsall Railways Station. Option 1 was to redesign the station create greater capacity and Option 2 was to promote the station as a transport hub but not seeking to enhance its capacity. At present the Chase (Walsall to Rugeley Trent Valley) rail line is undergoing electrification, once completed it will bring benefits such as faster and quieter journeys, more seats, and improved reliability. Electrification of the line may see services to destinations further afield introduced, which would in turn require the train station's capacity to be increased. Increasing capacity may require an additional platform to be created along with improvements to the station's passenger facilities. Due to the location of the rail station and its integration into the Saddler's Centre further consideration would be required as to how improvements could be achieved.

Walsall is fortunate to have a railway station located in the heart of the town, meaning visitors immediately arrive amongst the core retail and services zone. Enhancing the existing facility would impact positively on potential economic growth as well as allowing residents to receive enhanced rail services to destinations not currently available. As well as improvements to the rail station, option testing for the introduction of a rapid transit service between Walsall and Wolverhampton has been undertaken. Direct travel between Walsall town centre and Wolverhampton city centre is limited by public transport to bus travel. This journey of 35 minutes could be reduced to 15 minutes by introducing a rapid transit service between the two strategic centres. At present there is a heavy rail passenger line between Walsall and Wolverhampton which, although not officially closed has no regular passenger service operating on it (one direct 'parliamentary' train runs on Saturdays only at 06:38 from Wolverhampton). The route is utilised as a diversionary route for West Coast Mainline services when necessary.

Creating a new rapid transit service between Walsall and Wolverhampton would require an island platform to be formed alongside the current platform 1. The platform would allow full integration with the current rail station and nearby bus interchanges. Part of the existing rail station car park would be required to create the island platform and siding, however a large portion of the car park

would be obtained. A definitive route for this rapid transit service is still to be determined; however the stop within Walsall town centre would be located in the same position.

The Preferred Options Draft Plan has therefore progressed with Option 1 by allocating the car park at Station Street for the expansion of the railway station to accommodate future investment in rapid transit.

5. AAP Publication – March 2016

The AAP Publication did not alter the option to be taken forward. No further modifications were made during the Pre Submission Modification stage of November 2016.

6. Jerome Retail Park Option Development

Alongside the development of the AAP work has been undertaken to develop a potential solution for creating a new interchange facility on part of the Jerome Retail Park. This has included the development of several options resulting in the current preferred option which sees the creation of 10 bus stands and 3 bus layover bays.

Furthermore the Jerome Retail Park site has been assessed with regard to the land required to develop the preferred solution for Bradford Place. This has included an assessment of the impact on the retail, leisure and car parking land at the site as well as how the site could be reconfigured within the modified boundaries to deliver a site which provides retail opportunities as well as car parking.

List of Appendices

1. Summary of 2007 option appraisal work
2. Summary of 2007 option appraisal work - matrix
3. Cenotaph relocation file note
4. Station Street – discounting location file note
5. Bridgeman Street bridge – road lowering file note
6. Bridgeman Street bridge – rail track raising file note
7. Jerome Retail Park land reconfiguration file note
8. Jerome Retail Park land reconfiguration drawings

Rachel Telfer
April 2017

Appendix 1: Summary of 2007 option appraisal work

Bradford Place bus-interchange summary options to accompany scoring matrix

Option appraisal work from 2007 undertaken Walsall Council & Centro (now Transport for West Midlands)

Summary

Bradford Place is located at the southern end of the town centre (TC) of Walsall and acts as a strategic bus interchange facility within 5 minutes walking distance of St Paul's bus station, the railway station and the main shopping centre for Walsall. It has operated in this capacity for more than 30 years. The centre piece of Bradford Place is the Walsall War Memorial that dates from 1922 and was refurbished in 2004. The main purpose of the Bradford Place facility is for it to serve as a Public Transport (PT) interchange point for Walsall.

Bradford Place currently operates at a poor standard as a bus interchange facility. In keeping with the regeneration of Walsall TC, the bus-interchange facility at Bradford Place is being looked at to be redeveloped. Discussions within Walsall MBC and with Centro have identified a range of potential options for the redevelopment. This brief summarises the main proposal options for the redevelopment that are to be appraised.

This summary accompanies two appraisal matrices for the selected options for different criteria and serves as a source of information for comparing different options. These matrices can then be scored and marked for the different criteria in an independent manner, allowing a non subjective method of comparing different options. The options that score higher marks overall, are generally, the most attractive options for the bus interchange site in Walsall.

There are two appraisal matrixes. Options 1 to 10 in 'Long Term Options' Appraisal matrix are for the long term redevelopment of the bus interchange facility as a final solution. In the 'Short Term & Supplementary Options', there are 4 short term options and 3 supplementary options. These are intended to be looked as in interim solution to the public transport issues that currently affect Walsall TC, in particular from Bradford Place.

Three further supplementary options have been included in the short term matrix. These are not options in themselves but could be incorporated as part of another scheme. They are included as they could potentially assist the development of a bus interchange facility within Walsall. These are the opening up of 'The Bridge' and the improvement of access on 'Bridgman Street', under the railway line with a proposal to build an at grade level crossing over the railway line near to Bridgeman Street. These could all improve vehicular access with certain options but do have some limitations with cost implications and political sensitivities associated.

Long Term options Appraisal

1. Bradford Place 1 (Cenotaph relocated) (location 1 on map)

Redevelop Bradford Place interchange site and keep the current services operating from this location (location 1 on the map). At the moment Bradford Place is operating above capacity. This can be addressed with a more efficiently designed bus interchange layout. This will require the possible relocation of the Cenotaph depending on the final design plan, by approximately 10-15m. This option will provide capacity for 9 bus bays. There would be little alteration to the surrounding road infrastructure with this option.

CENTRO/ Walsall MBC initial comments for option

This is considered an attractive option by keeping the bus-interchange facility at Bradford Place. Relocation of the Cenotaph may be required but Bradford Place is considered the prime PT interchange location. Land acquisition is not a requirement with this option though some surrounding road improvements are needed in any case. Will be a politically sensitive scheme with the Cenotaph affected.

TWM comments

Keen to keep bus services operating from Bradford Place

2. Bradford Place 2 (Cenotaph base unit structure realigned)

Redevelop Bradford Place interchange site and keep the current services operating from the site (location 1 on the map). At the moment Bradford Place is operating above capacity. This can be addressed with a more efficiently designed bus interchange design. There will be a need to realign the base structure of the cenotaph to allow the interchange design to function efficiently, though the actual structure will not be affected. This option will provide capacity for 9 bus bays. There would be little alteration to the surrounding road infrastructure with this option.

CENTRO/ Walsall MBC initial comments for option

Will require the realignment of the Cenotaph base unit but offers the best location for PT interchange in the TC with the least engineering work of all the schemes for Bradford Place. General road infrastructure is in place (that requires some improvement in any case). No relocation or purchase of land makes this a very attractive option.

TWM comments

Keen to keep bus services operating from Bradford Place

3. Midland Road (location 3 on map)

Pedestrianise Bradford Place or create a public space in that area. Use Midland Road (location 3 on the map) as a one way lay-by bus mall interchange area in a southerly direction. This option could involved some 'land acquisition' at the southern end of Midland Road for the improvement of the turning angle for buses to be sufficient to make the option viable. At the moment the angle for turning buses is not ideal.

Midland Road is also residential. There may be a limitation on the number of bus stop points on Midland road due to the road operating as one way (possibly 5 lay-bys). Re-routing of the current services from Bradford Place to Midland Road would not be a significant problem though there is likely to be a capacity issue of buses on Midland Road.

CENTRO/ Walsall MBC initial comments for option

Further away from other PT sites. Possible limitations of bus interchange capacity as well due to width of road and the need for Midland Road to be one way. There may also be an issue for buses coming from Bridgman Street accessing Midland Road. Only major growth of the TC in a southerly direction would warrant this option.

TWM comments

Not keen as more remote from the town centre.

4. Soft touch option

Discontinue Bradford Place bus-interchange as it exists now. Pedestrianise Bradford Place to create a public space. Bus services currently using Bradford Place will be relocated and spread all over the town centre of Walsall, comparable in the way buses operate in Birmingham at the moment. There would have to be a significant amount of work done on the new routes into and out of Walsall with this option.

CENTRO/ Walsall MBC initial comments for option

There are some limitations to bus users with this option. No circular route exists in the TC to spread the bus stop locations through the TC as exists in Birmingham. Walsall also has an ageing population and by spreading bus stop locations, this may impact on bus users significantly to get into the TC. This option may potentially add to congestion in certain areas of the TC as a result of this option.

TWM comments

Not acceptable as a long term option without appropriate layover provision but as an interim measure for 9 months as a maximum it would be a serious consideration.

5. Jerome Retail Park (location 2 on map)

Redevelop Bradford Place interchange site and keep the current services operating from this site (location 1 on the map). At the moment Bradford Place is operating well above capacity. This can be addressed with a more efficiently designed bus interchange layout. There will need to purchase some land from the 'Jerome Retail Park' (location 2 on map). The Cenotaph will remain untouched with this option and bus stand capacity will not be an issue. There would be little alteration to the existing surrounding road infrastructure with this option.

TWM comments

Workable even with a DIRO facility in operation. Very clear benefits of keeping the interchange around this part of the town centre.

CENTRO/ Walsall MBC initial comments for option

A very attractive option. Will require some land acquisition from Jerome Retail Park but offers the best location for PT interchange in the TC. General road infrastructure is in place (that requires some improvement in any case). The Cenotaph would not be affected though some land acquisition is required from Jerome Retail Park.

6. Lower Hall Lane Car Park (location 4 on map)

Relocate Bradford Place bus interchange to the disused Lower Hall Lane Roof Top Car Park (location 4 on the map). This would allow the area around Bradford Place to be developed into a public space and be pedestrianised.

CENTRO/ Walsall MBC initial comments for option

Local road network is a concern for getting buses in and out of the location. The environment around the site has already been earmarked for other developments and a bus station may not be in keeping with these new developments. Bus services accessing Bridgman Street would have further to travel or possibly by a different route from what they do now. There would be room to accommodate 9 bus lay over stands but the local road network for bus use will need to be addressed. This could be an expensive option.

TWM comments

Very good penetration for buses into the town centre with this option. However, this could be a very difficult option to promote given its surroundings and future proposed developments. There may also be issues of bus routes into and out of the location.

7. Mountrath Street (location 5 on map)

Create a public space at around Bradford Place. Relocate services from Bradford Place to Mountrath Street (location 5 on map). Some of Mountrath Street has already been earmarked for flat development and this may not be in keeping with the new developments. The site size and layout would accommodate 6 or 7 bus bays. Bus services accessing Bridgman Street would also have further to travel.

CENTRO/ Walsall MBC initial comments for option

Lack of PT provision for area and distance from TC is a concern. The residential development planned along Mountrath Street may not be in keeping with the new development environment. The local road network is currently quite narrow and this would have to be addressed if there were to be significant bus movements in the vicinity of Mountrath Street.

TWM comments

Further away from the town centre. Similar concerns as for the Midland Road option.

8. Increase the capacity at St Paul's bus station with the 'Post Office' site

By increasing the capacity at St Paul's bus station (location 6 on map), Bradford Place could be redeveloped with a public space. There may be an opportunity for the 'Post Office' site to be purchased and used as an extension to St Paul's. There would be no shortage of bus stand capacity with this option. Bus services accessing Bridgman Street would have to use a different route to get to St Paul's. Serious consideration

needs to be given on the re-routing of bus services from the south of the TC to St Paul's bus station on the north side.

CENTRO/ Walsall MBC initial comments for option

This option would most probably involve taking over the land at the current Post Office site, which is due to be relocated in 3 years time. Although room exists to increase the size of St Paul's, the local roads surrounding St Paul's would most probably become saturated and cause significant congestion issues for both bus and car users. Air quality may also be an issue through site capacity and induced bus congestion from bus engine idling. This option would increase current journey times for bus services to the south of the TC, with distance and increased bus congestion on the northern side of town.

TWM comments

TWM view the positively any proposals to increase the capacity of St Paul's bus station. If a proportion of services from Bradford Place could be re-routed to St Paul's, less substantial works may be required with the Bradford Place interchange option.

9. Intown Row (location 7 on map)

Create a new 2 story bus station on the site at Intown Row (location 7 on the map). Bradford Place can be redeveloped in to a public space in some capacity. The site is small and on a gradient on the edge of the TC. Surrounding road network may not be suitable for large numbers of buses moving around the site. Bus services accessing Bridgman Street would have further to travel to access Intown Row.

CENTRO/ Walsall MBC initial comments for option

The Intown Row site is located to the east of the TC. Redevelopment of the site may not be possible due to the small size of the site and the gradient of the location. The existing road infrastructure will require significant upgrade in any case. Re-routing of bus services to the site may also cause increased congestion to the adjacent road network with this option.

TWM comments

The distance from the town centre and the topography of the area makes this a poor one for bus users. Not keen.

10. New bus management system

This new bus management system would involve providing lay over space around the Caldmore area and Vicarage Road, away from Bradford Place. There is presently no provision of lay over space for buses and this is one of the major causes of the poor operational capacity of Bradford Place. By providing this option as a solution, the operational capacity would greatly improve the whole facility as it exists now.

This would involve Walsall MBC taking ownership of the land that is occupied by Bradford Place and regulating bus movements on site and lay over time around Caldmore and at Bradford Place. The option would require getting the bus operators on board to make this work and 'policing' bus movements on and away from Bradford Place. There would be little alteration to the surrounding road infrastructure

required with this option except in routing buses to the lay over area a short distance away.

WRC/ Walsall MBC initial comments for option

Could be a cheaper option and potentially very successful for relieving congestion within Bradford Place. Needs to be looked at seriously.

CENTRO comments

Looks very interesting and CENTRO would be likely to be supportive of this option if it were to be looked at in further detail and support TWM and Walsall.

TWM comments

Positive views on this option if lay over space and regulation can be provided for the buses. Caldmore and Vicarage Road are close enough to Bradford Place to make this an interesting option to look into as a solution.

Short term options Appraisal

1. Re-profile Bradford Place (Short term option)

Some redesign of Bradford place as an interim solution is a consideration. This would be a sensible option if another location within Walsall is identified as the interchange site (location 1 on map). There would be little alteration to the surrounding road infrastructure with this option.

CENTRO/ Walsall MBC initial comments for option

Attractive option by keeping the bus-interchange facility at Bradford Place. Offers the best location for PT interchange in the TC. General road infrastructure is in place (that requires some improvement in any case). There may be a need to relocate the Cenotaph with this option and this may be a sensitive issue, but this may be avoidable if a short term option here were considered in detail.

TWM comments

Keen to keep bus services operating from Bradford Place

2. Midland Road (short term option)

It would be possible to use Midland Road (location 3 on the map) as a temporary one way lay-by bus interchange area in a southerly direction. At the moment the angle for turning buses is not ideal and Midland Road is also residential. There is a limitation on the number of bus stop points on Midland road due to the road operating as one way (possibly 5 lay-bys). Re-routing of the current services from Bradford Place to Midland Road would not be a significant problem though there is likely to be a capacity issue of buses on Midland Road.

CENTRO/ Walsall MBC initial comments for option

Further away from other PT sites. Possible limitations of bus interchange capacity as well due to width of road and need for Midland Road to be one way. There may also be an issue for buses coming from Bridgman Street accessing Midland Road. Only major growth of the TC in a southerly direction would warrant this option.

TWM comments

Not keen as more remote from the town centre.

3. Soft touch option (short term option)

As a short term option, discontinue Bradford Place bus-interchange as it exists now. This would allow works to start on Bradford Place (whether this be for the creation of a public space or an interchange site). Bus services currently using Bradford Place will be relocated and spread all over the town centre of Walsall.

CENTRO/ Walsall MBC initial comments for option

There are some limitations to bus users with this option. No circular route exists in the TC to spread the bus stop locations through the TC as exists in Birmingham. Walsall also has an ageing population and by spreading bus stop locations, this may impact on bus users significantly to get into the TC. This option may potentially add to congestion in certain areas of the TC as a result of this option.

TWM comments

Not acceptable as a long term option without appropriate layover provision but as an interim measure for 9 months as a maximum, it is a serious consideration. Not to be ruled out.

4. New bus management system

This new bus management system would involve providing lay over space around the Caldmore area and Vicarage Road, away from Bradford Place. There is presently no provision of lay over space for buses and this is one of the major causes of the poor operational capacity of Bradford Place. By providing a solution to this would greatly improve the whole facility as it exists now.

This would involve Walsall MBC taking ownership of the land that is occupied by Bradford Place and regulating bus movements on site and lay over time around Caldmore and at Bradford Place. The option would require getting the bus operators on board to make this work and 'policing' bus movements on and away from Bradford Place.

WRC/ Walsall MBC initial comments for option

Could be a cheaper option and potentially very successful for relieving congestion within Bradford Place. Needs to be looked at seriously.

TWM comments

Positive views on this option if lay over space and regulation can be provided for the buses. Caldmore and Vicarage Road are close enough to Bradford Place to make this an interesting option to look into as a solution.

Supplementary options Appraisal

5. Opening up of 'The Bridge' (supplementary option)

This option would involve allowing buses through the pedestrianised zone beyond Bradford Place, known as 'The Bridge' (location 8 on the map). This would connect the two sides of Walsall with road access for the 1st time since the late 1990's. This is not an alternative in its own right but could be used in conjunction with another scheme option.

CENTRO/Walsall MBC initial comments for option

This may be politically sensitive. The Bridge was closed off to traffic in the late 1990's and this has been universally popular in pedestrianising Walsall TC. A problem also exists as to where buses would be relocated if this was opened up again. As a solution to redeveloping Bradford Place as an interchange site, this may not be suitable. Careful consideration is needed for the schemes that this could be used in conjunction.

TWM comments

Not realistically politically viable but would be a very good way to reconnect the two sides of Walsall together again and assist bus users.

6. Bridgman Street engineering works (supplementary option)

The underpass on Bridgman Street is currently in a poor state (location 9 on the map). This option would entail engineering works to allow 'Double Decker' buses to pass under the bridge and towards Bradford Place. There are 3 bus services that use this route of the 18 routes that operate from Bradford Place. This option could be incorporated as part of a wider scheme that involves Bradford Place and would allow more buses in from the west of Walsall.

CENTRO/ Walsall MBC initial comments for option

This could be a costly exercise. If Bradford Place is to remain operating as it currently does, then this could be a way of getting more services into Bradford Place other than just single decker buses as happens now. There are only a few services that use this route at the moment. If more bus services were to come into Walsall from the west, then this engineering scheme is a consideration. Cost is the main issue with this option that could be better spent on other improvements.

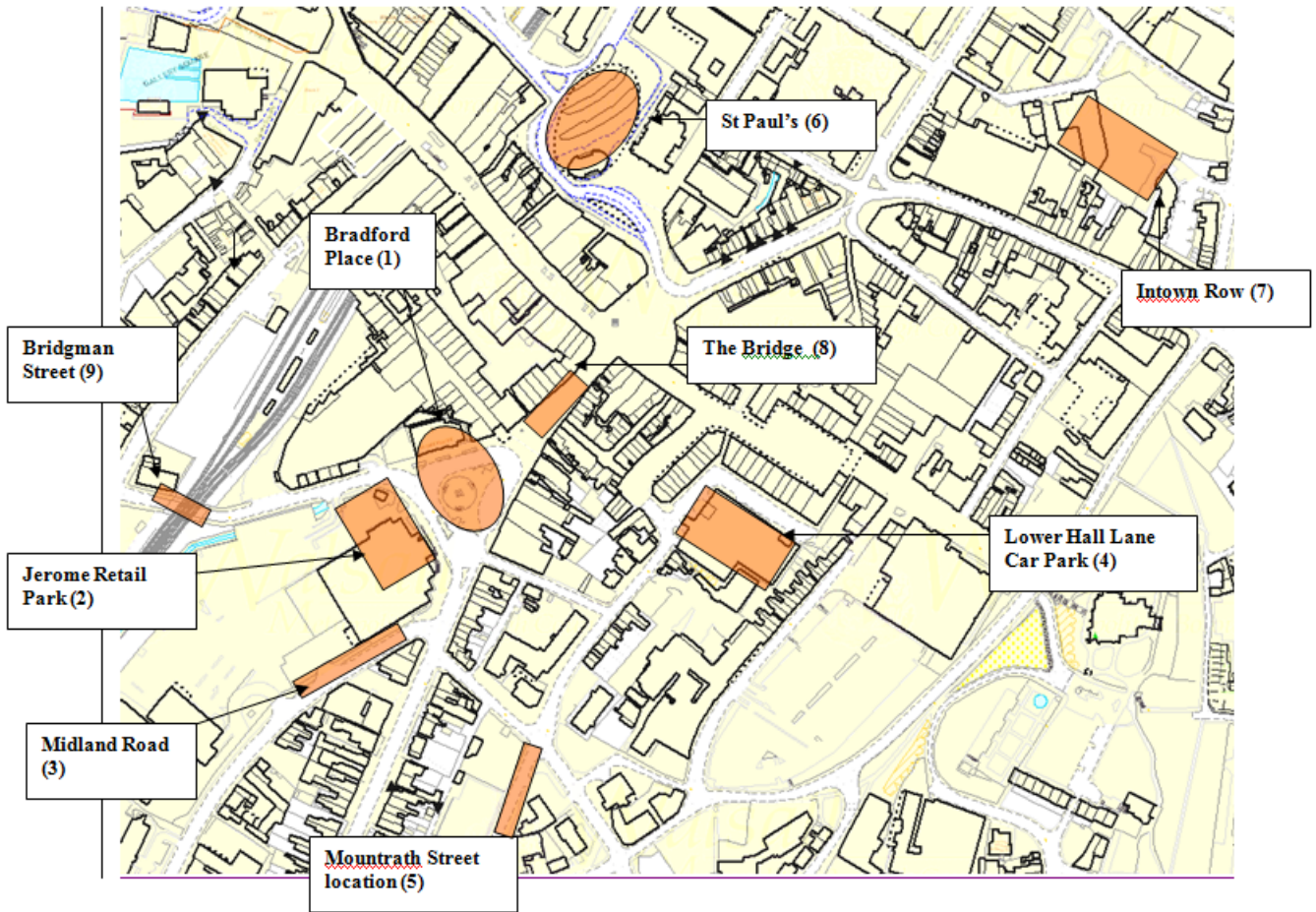
7. 'At Grade' crossing near Bridgeman Street over railway

Explore the feasibility of designing a new 'At Grade Crossing' level crossing to alleviate the cost of significant engineering costs under Bridgeman Street and allow 'Double Decker' buses access from the west of Walsall. Extensive talks with Network Rail and Centro would be required for this option to be taken forward. The location of the crossing would be to the north of Bridgeman Street.

CENTRO initial comments for option

Level Crossing. In our opinion this is a complete non starter. Having looked at the potential for new level crossings on much quieter railways than this without any success, this is not a scheme to take forward. It is virtually impossible to get new level

crossings approved these days and it is ill advised that we should invest time or effort in this option.



Appendix 2: Summary of 2007 option appraisal work - matrix

Sites analysed >		Long Term	Long Term	Long Term	Long Term	Long Term	Long Term	Long Term	Long Term	Long Term	Long Term
		1. Bradford Place (Cenotaph relocation)	2. Bradford Place (Cenotaph realignment)	3. Midland Road 'Bus Mall'	4. 'Soft Touch' Option	5. Jerome Retail Park	6. Lower Hall Lane Car Park	7. Mountrath Street	8. St Paul's Bus Station (Post Office site)	9. Intown Row	10. 'Advanced System'
LOCATIONAL REQUIREMENTS SCORING SYSTEM IDENTIFIED FOR EACH INDIVIDUAL CRITERIA. HIGHER MARKS SUGGEST MORE POSITIVE IMPACTS OF THE SCHEME.											
A - Site Area											
1	What are the likely numbers of bus bays/boarding points achievable from the site? (Score- , 9+ =3, 7-8 bays= 1.5, 0-6 bays= 0)	3	1.5	3	3	3	3	1.5	1.5	3	3
2	How well can the site be safely integrated with public space in the locality? (Score- Well=3, Potentially=1.5, Poorly=0)	3	3	1.5	1.5	3	1.5	0	3	0	3
3	Is there a need for land acquisition with the proposed site? (Score- No=3, Possibly=1.5, Yes=0)	3	3	0	3	0	0	1.5	0	0	3
B - Proximity to Walsall Town Centre											
4	Can the site offer a highly visible facility to the public in the Town Centre (TC)? (Score- Yes=7, Possibly= 3.5, No= 0)	7	7	3.5	3.5	7	0	0	7	0	7
5	How does site integrate with other Public Transport (PT) facilities and the Town Centre? (Score- Well =7, Moderately= 3.5, Poorly=0)	7	7	0	3.5	7	3.5	0	7	0	7
6	Can the scheme be safely designed around pedestrians, bus users and other surrounding land use developments? (Score- Yes=7, No=0)	7	7	7	7	7	7	7	7	7	7
C - Good Vehicular Access to Public Transport (PT) interchange and infrastructure											
7	Does site offer the opportunity to improve journey time of bus services? (Score- Yes= 5, No= 0)	5	5	5	0	5	0	0	0	0	5
8	Can site location reduce or mitigate TC congestion? (Score- Yes= 5, Potentially= 2.5, No= 0)	5	5	5	0	5	0	5	0	0	5
9	Would further highway improvements be required locally to the site? (Score- None=5, Minor=2.5, Yes=0)	5	5	0	0	0	0	0	0	0	0
10	Does site offer capacity/opportunity to increase PT use and improve the current user 'offer'? (Score- Yes=5, No= 0)	5	0	0	0	5	0	0	0	0	5
D - Timeframe of site 'Availability'											
11	Is the scheme 'deliverable' in the short (1-5 years), medium (3-8 years) or long (7+ years) term? (Score - Short= 10, Medium= 5, Long= 0)	5	5	5	10	0	0	5	0	0	5
E - Suitability for sustainable 'Site Usage'											
12	Is the site suitable with surrounding infrastructure to offer a good interchange between the TC and site for pedestrians, bus users and rail users? (Score - Yes= 5, No= 0)	5	5	0	0	5	0	0	5	0	5
13	Is there potential to include the private sector as an integral partner to the scheme development? (Score - Yes= 5, No=0)	0	0	0	0	5	5	0	5	5	0
F – Design Criteria											
What opportunities exist for the creation of a 21st Century bus interchange within Walsall in regard to-											
14	Integration of urban design?	5	5	0	0	5	2.5	0	5	0	5
15	Design of a safe environment and bus station?	5	5	2.5	2.5	5	2.5	2.5	5	2.5	5
16	Increasing the profile of Walsall? (Score- Excellent= 5, Some= 2.5, None=0)	2.5	2.5	0	0	5	0	0	2.5	0	2.5
G – Policy											
How well can the scheme support/ develop the following national/local policy development for-											
17	Improvement of air quality?	0	0	1.5	3	0	0	0	1	0	0
18	Promotion of PT over car use?	3	3	1.5	0	3	1.5	0	1.5	0	3
19	Encouragement of wider economic development?	3	3	1.5	1.5	3	1.5	0	1.5	0	3
20	Local issues (conservation, local importance)?	0	0	0	1.5	3	1.5	0	0	0	1.5
21	The development and contribution of the LTP? (Score- Well= 3, Moderately= 1.5 , Not= 0)	1.5	1.5	1.5	3	0	0	1.5	0	0	1.5
Total Score as % - maximum of 100 points = 100%		80	73.5	38.5	43	76	29.5	24	51	17.5	76.5

Appendix 3: Cenotaph relocation file note



Regeneration Directorate
(Transportation)
2nd Floor, The Civic Centre, Darwall Street,
Walsall, WS 1 1DG, West Midlands
Tel : 01922 65 0000
Fax : 01922 62 3234

Project Ref: Bradford Place Bus Interchange
By: RT
Date: 12.09.16
Checked : Approved :
Title: Walsall Cenotaph Relocation

Filenote

Bradford Place Bus Interchange Project – Potential of Relocating Walsall Cenotaph

Walsall cenotaph is located at Bradford Place within a triangular intersection of Bridgeman Street and Bradford Street. Surrounded by buses, taxis and traffic accessing local businesses, it is far from an ideal location for a cenotaph to be located. However, its location is of significance. In early 1916 Zeppelin airships visited Walsall and Wednesbury, mistaking the Black Country towns for the intended target of Liverpool. One of the bombs dropped landed directly outside the Science and Art Institute (now named Globe House) in Bradford Place claiming the lives of three people including Walsall's Lady Mayoress. After the First World War the location of cenotaph was chosen to be the crater made by the bomb outside the Science and Art Institute. The monument itself sits surrounded by a landscaped area with three sets of steps featuring sculpted handrails decorated with poppies. Both the cenotaph and the steps are separately listed structures.



Left Photo: Location of Walsall Cenotaph surrounded by bus stands.

Right Photo: Walsall Cenotaph monument and surrounding landscaped area

It had been considered that if the cenotaph were to be relocated to a more fitting location then the space could be re-imagined to create a new bus interchange.

On 24 August 2016 Rachel Telfer spoke with John Somers, Building Conservation Officer, to determine his opinion of relocating the cenotaph. John informed he would highly oppose to the relocation of the cenotaph due to the significance of its location and the fact that if it were not located there its context would be lost. In addition he also reiterated that not only is the cenotaph listed but the steps up to it are also listed as a separate structure.

Upon reflection of the information gained I do not feel it would be appropriate for the cenotaph to be relocated. In addition, I do not feel that even without the cenotaph, there would not be enough space to create an adequate facility.

Creating a new facility to the south of Bradford Place would allow the removal of traffic from around the cenotaph allowing public realm improvements in the location to be further improved and create a more fitting location for people to visiting the cenotaph to enjoy. This was strongly supported by John.

Rachel Telfer
September 2016

Appendix 4: Station Street – discounting location file note



Economy & Environment
(Strategic Transportation)
2nd Floor, The Civic Centre, Darwall Street,
Walsall, WS 1 1DG, West Midlands
Tel : 01922 65 0000
Fax : 01922 62 3234

Project Ref: Walsall Town Centre AAP
By: RT
Date: 21.02.17
Title: Station Street Bus Station Option

Filenote

Walsall Town Centre AAP – Station Street Bus Station Option Appraisal

Introduction

Walsall Council is at present going through the process of adopting an Area Action Plan (AAP) for Walsall town centre.

Within the AAP are proposals to create a new bus interchange facility on part of the existing Jerome Retail Park. This location has been determined as the most suitable location to create a facility which will meet the needs of existing and future bus services in this area of Walsall town centre.

The AAP will be subject to a Public Examination in March 2017 and therefore we are at present collating the relevant evidence to support the AAP at examination.

It is anticipated that the owners of Jerome Retail Park will object to the proposed use of part of the retail park for a new interchange. One of their comments has been that there is not enough information provided regarding locations which have been discounted. One initial idea was to locate a new interchange facility close to Walsall train station (in Station Street). However, this would require bus services to travel underneath the railway along Bridgeman Street. The height restrictions of this bridge would mean that double-decker buses would not be able to travel to the new facility from the Bradford Street site of Walsall town centre. Creating a facility that could only allow single decker buses to use it would be detrimental to the future of the facility's functionality.

Other files notes have been prepared to explore lowering the road under the bridge as well as raising the tracks.

If it were to be a feasible option to lower/raise the tracks, a new facility along Station Street would not be viable due to the following design constraints.

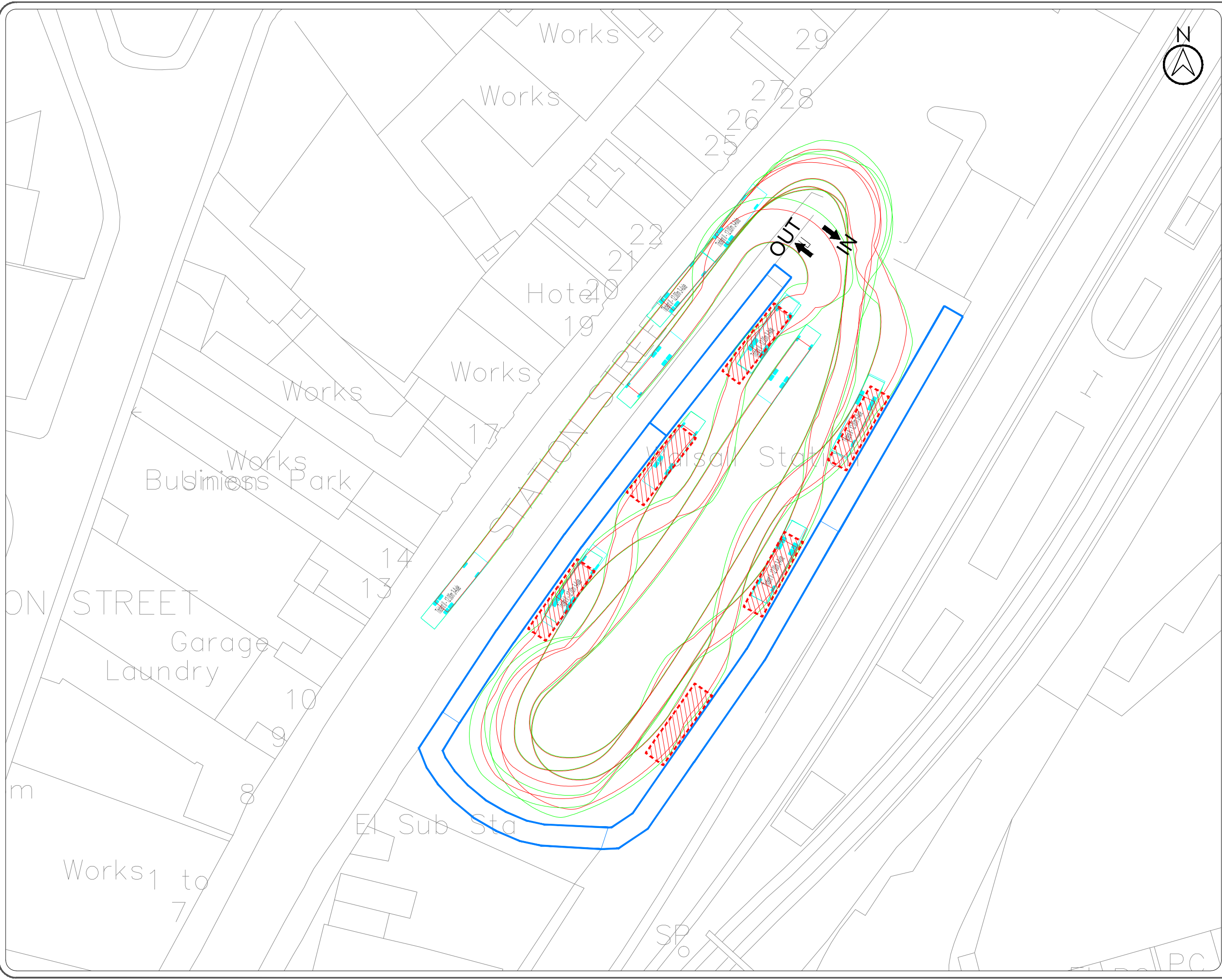
Comments below are based on the design work undertaken by Transport for West Midlands (TfWM), in particular the drawing 'Alternative Location Station Street'.

1. Facility provides 6 stands; current facility at Bradford Place has 6 stands plus 1 drop off. Therefore provides no capacity improvements. No potential to expand site and increase capacity. No layover space available.
2. Adjacent to rail station providing excellent interchange possibilities, however more isolated location further from main shopping area.

3. Utilises current rail station car park, resulting in loss of complete parking area adjacent to the rail station. On street parking would also be lost. High portion of on street parking along Station Street is designated disabled; this too would be most affected.
4. Very tight left hand turn out of bus station facility, little scope to alter road geometry to accommodate such movements. Station Street is a well used route for taxis and cars as well as HGVS accessing Marks and Spencer, therefore buses performing difficult manoeuvres may create issues and delays along route.
5. Aspirations for Station Street via Walsall Council master planning may not include the desire to have a bus station opposite future developments.
6. Creation of further vehicular conflicts along Station Street, especially within the area outside of the entrance/exit to platform 1 of the rail station.

In addition to these comments, it is the long-term ambition of the Council to have tram-train services operating between Walsall and Wednesbury (and possibly beyond) to integrate with the Midland Metro network. The terminus for such service would most likely be adjacent to the rail station and require land from the current car park area to turn platform 1 into an 'island platform', creating adequate space for the tram-trains to terminate.

Rachel Telfer
February 2017



Notes / Legend:

Replacement bus interchange facility for Bradford Place.

- Bradford Place (6 stands)
- Station Street (6 Stands)

PROS

- 6 Shelters
- Interchange with rail station.
- No Crossings in bus station.

CONS

- Low bridge nearby restricts access
- Significant loss of on street parking
- Tight left hand turn out of bus interchange
- Loss of all rail station parking

Proposed Kerline

Proposed Bus Bay

REV	DESCRIPTION	INIT	DATE
A	X	X	X



SITE:
Walsall
Bradford Place

DRAWING TITLE:
Alternative Location
Station Street

DRAWING REFERENCE:
WALS_BP_01

DRAWING FILE LOCATION:
R:\OS Maps\St Pauls & Bradford Place.dwg

SCALE AT A3:	DATE:	DRAWN:	REVISION:
1:500	Nov '16	NB	-

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West Midlands Combined Authority.
100019543. 2016.

West Midlands Combined Authority
16 Summer Lane, Birmingham, B19 3SD
Tel: 0345 303 6760
www.westmidlandscombinedauthority.org.uk

Appendix 5: Bridgeman Street bridge – road lowering file note



Economy & Environment
(Strategic Transportation)
2nd Floor, The Civic Centre, Darwall Street,
Walsall, WS 1 1DG, West Midlands
Tel : 01922 65 0000
Fax : 01922 62 3234

Project Ref: Walsall Town Centre AAP
By: RT
Date: 23.02.17
Title: Bridgeman Street Bridge

Filenote

Walsall Town Centre AAP – Bridgeman Street Bridge Road Lowering

Introduction

Walsall Council is at present going through the process of adopting an Area Action Plan (AAP) for Walsall town centre.

Within the AAP are proposals to create a new bus interchange facility on part of the existing Jerome Retail Park. This location has been determined as the most suitable location to create a facility which will meet the needs of existing and future bus services in this area of Walsall town centre.

The AAP will be subject to a Public Examination in March 2017 and therefore we are at present collating the relevant evidence to support the AAP at examination.

It is anticipated that the owners of Jerome Retail Park will object to the proposed use of part of the retail park for a new interchange. One of their comments has been that there is not enough information provided regarding locations which have been discounted. One initial idea was to locate a new interchange facility close to Walsall train station (in Station Street). However, this would require bus services to travel underneath the railway along Bridgeman Street. The height restrictions of this bridge would mean that double-decker buses would not be able to travel to the new facility from the Bradford Street site of Walsall town centre. Creating a facility that could only allow single-decker buses to use it would be detrimental to the future of the facility's functionality.

One suggestion by the owners of Jerome Retail Park is that the rail track could be raised or the road lowered to accommodate double-decker buses through the bridge. The bridge is already subject to flooding and lowering it would further be exacerbated by the presence of the nearby Ford Brook. The current height restriction on the structure is 3.2 m while the minimum clearance required for double-decker buses is 4.3 m. Therefore, a total increase of clearance of 1.1 m is required.

To assist with preparing information for the AAP Public Examination the following information has been collated to provide commentary regarding the lowering of the road at this location.

Comments

Visual examination shows that water is currently seeping through the road surface. Drainage is present on the road but it is not functioning adequately. This indicates that the existing road surface is at the

approximate level of the water table and any lowering of the road surface would likely lead to deeper standing water on the road.

The structure currently experiences severe flooding, with regular occurrences to the degree that requires cars to be winched out from under the bridge. Further lowering of the road surface would likely lead to an increase in the severity and occurrence rate of the flooding.

Due to the substantial decrease in road level, it is probable that significant stabilisation and strengthening works might be required to ensure that the stability of foundations and the concrete columns supporting the bridge superstructure is not affected.

The Ford Brook runs adjacent to Bridgeman Street, with the brook less than a meter below the lowest point of Bridgeman Street and, once the road is lowered, the brook would likely be a source of increased flooding risk.

Required Works

Lowering of the road would require the following works:

- Lowering the road surface and the sloped approach to the structure in order to maintain the current road gradient. The road is already considerably lower than the surrounding connecting roads. Lowering Bridgeman Street will require substantial works to the connecting roads to maintain an acceptable road profile. Greater work will be required on the east side of the structure approaching the Walsall Urgent Care Community site, as well as potential reconfiguration of the junctions of Station Street, Navigation Street and Long Street.
- Stabilisation of structure foundations will be required due to large excavation being necessary adjacent to the piers and the embankments of both adjacent roads. There is considerable risk to undermining the pier foundations. It is likely that the current foundations are the original foundations installed circa 1870.
- Investigations into the size, form and condition of pier foundations would need to be undertaken before the effects of road lowering can be fully assessed.
- Installation of drainage will be required. As the existing road is approximately level with the water table, lowering the road will lead to a high likelihood of the road flooding and standing water being present on the road most of the time without an adequate drainage system.

The two likely drainage methods for the structure are:

1. Installation of passive drainage allowing the water to flow away from the structure would be preferable but would be difficult to install due to the road likely being lower than likely outlets such as Walsall Canal and Ford Brook. Thus it is likely that passive drainage would need to be installed as far as the River Tame. It is unknown if there is current infrastructure such as a sewer network that this drainage system could be appended to, though it is deemed unlikely due to the road being much lower than surrounding buildings.

2. If passive drainage cannot be installed than active methods would need to be considered. This would likely take the form of a 24 hour operating pump which will impose considerable upkeep costs and maintenance requirements.

Risks

Due to the poor drainage and road profile at the site, the following risks must be considered and mitigated in any proposed scheme:

- Standing water and road flooding risk: due to the current level of the road being approximately level to the water table any lowering of the road will lead to an increased likelihood of the road flooding and standing water being present on the structure. This would lead to an increased risk of accidents or injury and would require drainage mitigation measures. Due to the new road layout probably being below the water table and below Ford Brook extensive drainage work would be required. Indications that the road is at a similar level to nearby outlets such as the canal and Ford Brook therefore a suitable hydraulic gradient for passive drainage cannot be established. Drainage would probably require a 24 hour operating pump to provide drainage which would be prone to high maintenance costs and a high likelihood of being overwhelmed in periods of heavy rainfall. The standing water on the road would cause an increased risk of traffic accidents as well as increased periods of closure leading to disruption of traffic along the road and surrounding routes.
- Structural risks to the foundations: due to large excavation being necessary adjacent to the piers there is considerable risk of undermining to the pier foundations. Investigations into their size, form and condition would need to be undertaken before the effects of road lowering can be fully assessed. It is very likely that reinforcement or stabilisation works would need to be undertaken to prevent the pier foundations from being undermined during the road lowering process.
- Risks to traffic: the road alignment is currently deemed to be poor. Investigation and mitigation would need to be considered to ensure that there is not an unacceptable increase in risk for traffic using the road. The road already has a curved profile with a considerable gradient. Increasing the gradient further will increase the risk of traffic accidents. Potentially, the route could be signalled to create a one way system to allow safer passage of traffic, however this would cause further delay to traffic flow along Bridgeman Street.

Recommendations

The lowering of Bridgeman Street to accommodate double-decker busses under the bridge would include several key works:

- Lowering Bridgeman Street;
- Adapting the profile of roads connecting to Bridgeman Street;
- Stabilisation of the foundations of the piers;
- Installation of drainage for the structure.

The scheme to lower Bridgeman Street Bridge has several key risks:

- The current road level being approximately equal to the water table makes further lowering of the road impractical. Any reduction in road level will likely lead to standing water on the road without considerable mitigation measures, such measures would lead to large amount of maintenance being required with considerable time and financial cost.
- The foundations to the bridge columns are unknown and would be at significant risk from undermining if the road was lowered further. The foundations will require significant investigation and work to prevent a reduction in integrity of the structure.
- The change in road profile would probably increase the risks to traffic using the road.

These works are considerably larger than a typical road realignment project of this scale. Whilst costs cannot be estimated at this stage with the information provided, it is likely that they would be much higher than a typical road realignment project of similar scale.

Given the concerns for the viability of the scheme (especially the drainage), the probable increased risk to the structure foundations, the probable increased risk to traffic using the road and considerable costs of implementation it is not recommended that this scheme is implemented whilst alternative proposals for the bus routes are available.

Rachel Telfer
February 2017

Appendix 6: Bridgeman Street bridge – rail track raising file note



Economy & Environment
(Strategic Transportation)
2nd Floor, The Civic Centre, Darwall Street,
Walsall, WS 1 1DG, West Midlands
Tel : 01922 65 0000
Fax : 01922 62 3234

Project Ref: Walsall Town Centre AAP
By: RT
Date: 31.01.17
Title: Bridgeman Street Bridge

Filenote

Walsall Town Centre AAP – Bridgeman Street Bridge

Comments from Network Rail officers
Martin Tiller – Sponsor – LNW Route
Matt Brown – Project Manager – LNW (South)

Introduction

Walsall Council is at present going through the process of adopting an Area Action Plan (AAP) for Walsall town centre.

Within the AAP are proposals to create a new bus interchange facility on part of the existing Jerome Retail Park. This location has been determined as the most suitable location to create a facility which will meet the needs of existing and future bus services in this area of Walsall town centre.

The AAP will be subject to a Public Examination in March 2017 and therefore we are at present collating the relevant evidence to support the AAP at examination.

It is anticipated that the owners of Jerome Retail Park will object to the proposed use of part of the retail park for a new interchange. One of their comments has been that there is not enough information provided regarding locations which have been discounted. One initial idea was to locate a new interchange facility close to Walsall train station (in Station Street). However, this would require bus services to travel underneath the railway along Bridgeman Street. The height restrictions of this bridge would mean that double-decker buses would not be able to travel to the new facility from the Bradford Street site of Walsall town centre. Creating a facility that could only allow single decker buses to use it would be detrimental to the future of the facility's functionality.

One suggestion by the owners of Jerome Retail Park is that the rail track could be raised or the road lowered to accommodate double decker buses through the bridge. The bridge is already subject to flooding and lowering it would further be exacerbated by the presence of the nearby Ford Brook.

To assist with preparing information for the AAP Public Examination Network Rail were asked to comment on the feasibility of undertaking track raising at this location.

Network Rail Comments:

Two immediate options are to increase the soffit height and/or a combination with road lowering.

There are existing issues with highway drainage under this structure, and given the existing road vertical alignment is poor, would lead to thoughts that further alteration would not be preferable. Road lowering has previously been undertaken. There is no information regarding abutment or pier stability, it would be difficult to estimate this work, a conservative estimated costs would be approximately £5-7m assuming the highway alignment could be agreed with Walsall Council. Assuming a full deck replacement and substructure strengthening/reconstruction.

Soffit height increase potentially could be engineered from a modern deck design, however this would need to be investigated further, and you may gain 100-200mm extra, dependent upon the positioning of the piers.

Regarding any effect of increasing track level, it is suggested that anything over 25mm would lead to significant platform reconstruction to all three platforms. Any lift in the region of over 300mm would probably drive station reconstruction and issues with contact wire gradient within Walsall tunnel. Costs for these works would be upwards of £15m.

In addition, track possession arrangements would be required, these would be complicated and expensive, with a long lead time, given the amount of freight and passenger traffic travelling over this bridge. As this is an electrified railway we would also need to consider the impact on the OLE equipment.

NB. It should be noted that the costs quoted in this document are initial estimates, with a high probability actual costs would be considerably higher.

Rachel Telfer
January 2017

Appendix 7: Jerome Retail Park land reconfiguration file note



Economy & Environment
(Strategic Transportation)
2nd Floor, The Civic Centre, Darwall Street,
Walsall, WS 1 1DG, West Midlands
Tel : 01922 65 0000
Fax : 01922 62 3234

Project Ref: Walsall Town Centre AAP
By: RT
Date: 19.04.17
Title: Jerome Retail Park

Filenote

Walsall Town Centre AAP – Jerome Retail Park

Introduction

Walsall Council is at present going through the process of adopting an Area Action Plan (AAP) for Walsall town centre.

Within the AAP are proposals to create a new bus interchange facility on part of the existing Jerome Retail Park. This location has been determined as the most suitable location to create a facility which will meet the needs of existing and future bus services in this area of Walsall town centre.

The AAP will be subject to a Public Examination in March 2017 and therefore we are at present collating the relevant evidence to support the AAP at examination.

It is anticipated that the owners of Jerome Retail Park will object to the proposed use of part of the retail park for a new interchange. They have commented that the AAP does not provide clarity on the delivery of the scheme as well the impacts upon the site.

To assist in assessing how the site could be reconfigured to best make use of the remaining land available and optioneering assessment was undertaken.

Please refer to the appended plans.

Jerome Retail Area – Land Affected

This plan highlights the initial estimations of the land required to enable the new bus facility to be built. It shows that a small portion of the cap park would be required resulting in a loss of 24 spaces.

Jerome Retail Area – Option 1

This plan highlights the land which could be available for retail on the site without impacting further on car parking. It assumes that the services areas for retail units would be similar to the present conditions. The plan shows how units could have front facing aspect onto the road, overlooking the new bus station. The entrance to the car park would be in a similar location.

Jerome Retail Area – Option 2

This plan highlights the land which could be available for retail on the site, with a view to maximising the floor space as much as possible without impacting too much on car parking. It assumes that the services areas for retail units would be similar to the present conditions. The plan shows how units

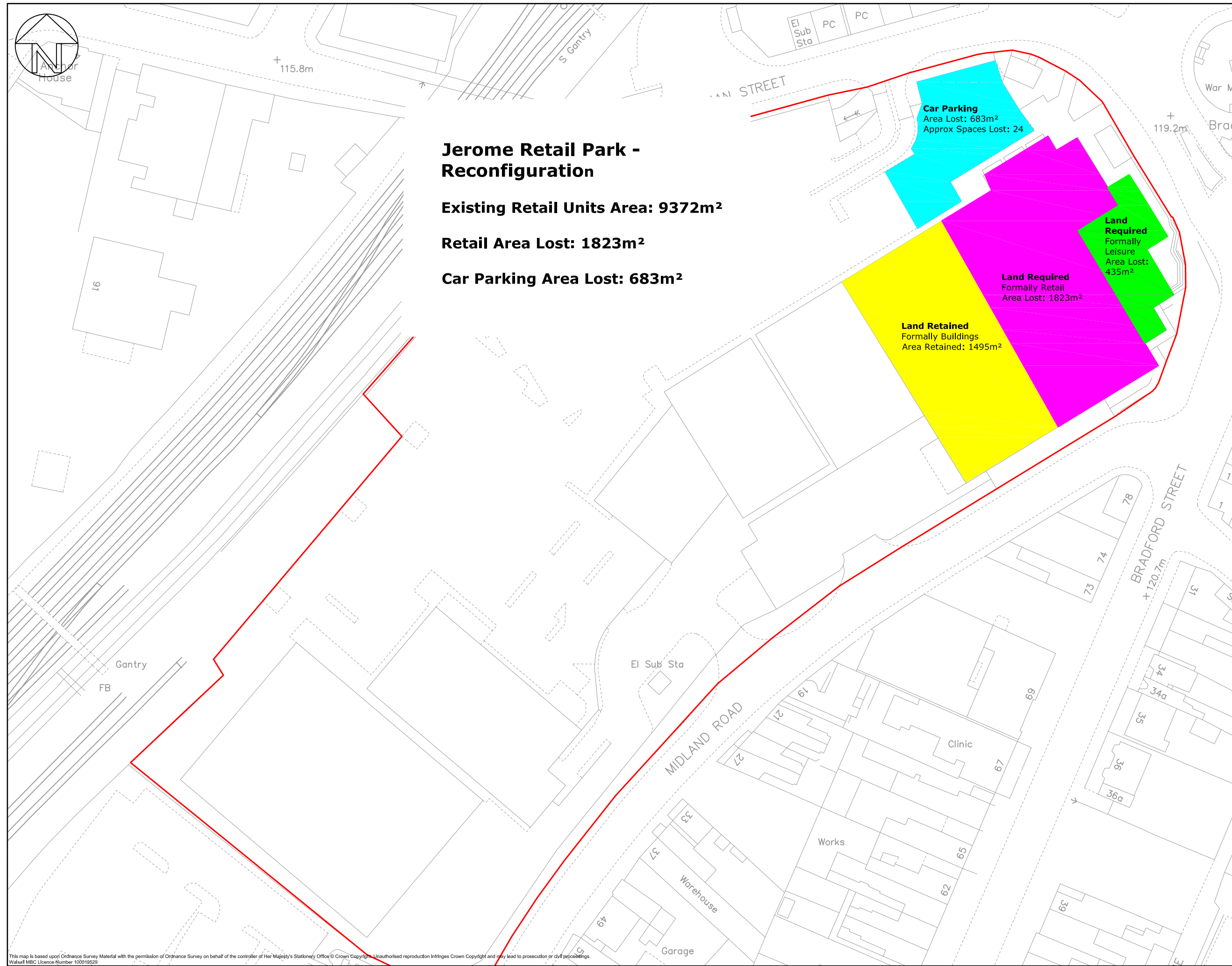
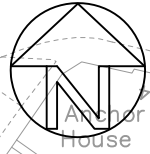
could have front facing aspect onto the road, overlooking the new bus station. The entrance to the car park would be in a similar location.

Both options have been developed to provide as much retail floor space without impacting upon the car parking available to the future retail units.

It should be noted these plans are an indicative assessment of the land required and land effected as part of the current proposed improvements for a new facility to replace Bradford Place Interchange. The potential solutions presented are for initial illustrative purposes and should not be taken as final proposals to reconfigure the site.

Rachel Telfer
April 2017

Appendix 8: Jerome Retail Park land reconfiguration drawings



Jerome Retail Park - Reconfiguration

Existing Retail Units Area: 9372m²

Retail Area Lost: 1823m²

Car Parking Area Lost: 683m²






Car Parking
Area Lost: 683m²
Approx Spaces Lost: 24

Land Required
Formally Leisure
Area Lost: 435m²

Land Required
Formally Retail
Area Lost: 1823m²

Land Retained
Formally Buildings
Area Retained: 1495m²

NOTES:
This plan is an indicative assessment of the land required and land effected as part of the current proposed improvement for a new facility to replace Bradford Place bus interchange.

-  Site boundary
-  Land required for project - retail
-  Land required for project - leisure
-  Former car park land required
-  Land unaffected and retained

A	17		RT	MC	MC
Rev	Date	Detail	Issue	Created	Approved

Project: Bradford Place Bus Interchange

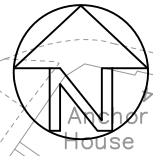
Title: Jerome Retail Park Indicative Land Affected Plan

Design: RT	CAD: RT
CHK'd: MC	App'd: MC
Date: 18.04.17	Scale: NTS



No. Rev.

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Jerome Retail Park - Reconfiguration

Existing Retail Units Area: 9372m²

New Retail Unit Area: 7634m²

Retail Area Lost: 1823m²

Leisure Area Lost: 435m²

Retail Unit Area: 3912m²

Retain Existing Units
Total Area: 3722m²

Land Required
Area Lost: 683m²
Approx Car Parking
Spaces Lost: 24

Land Required
Formerly
Leisure
Area Lost:
435m²








Land Required
Formerly
Retail Area
Lost: 1823m²

Opportunity for frontages facing highway/interchange

Proposed Car Park
Entrance/Exit

NOTES

This plan is an indicative assessment of the land required and land effected as part of the current proposed Improvement for a new facility to replace Bradford Place bus interchange. The potential solutions presented are only for illustrative purposes.

-  Site boundary
-  Land required for project - retail
-  Land required for project - leisure
-  Former car park land required
-  Land unaffected and retained
-  Unaffected retail units
-  Proposed entrance/exit to car park

Rev	Date	Detail	Drawn	Checked	App'd
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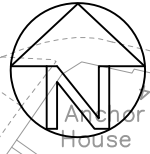
Project: Bradford Place Bus Interchange

Title: Proposal Option 1 Jerome Retail Park Indicative Land Affected Plan

Design: RT	CAD: RT
CHK'd: MC	App'd: MC
Date: 18.04.17	Scale: NTS



No. Rev.



Jerome Retail Park - Reconfiguration

Existing Retail Units Area: 9807m²

Retail Area Lost: 1823m²

Leisure Area Lost: 435m²

New Retail Unit Area: 8374m²

Retail Unit Area: 8374m²

Car parking lost 36 spaces (1019m²)

Former retail unit land reallocated to parking (363m²)

Land Required Area Lost: 683m² Approx Car Parking Spaces Lost: 24

Land Required Formerly Leisure Area Lost: 435m²

Land Required Formerly Retail Area Lost: 1823m²

Opportunity for frontages facing highway/interchange

Proposed Car Park Entrance/Exit

NOTES
This plan is an indicative assessment of the land required and land effected as part of the current proposed Improvement for a new facility to replace Bradford Place bus interchange. The potential solutions presented are only for illustrative purposes.

- Site boundary
- Land required for project - retail
- Land required for project - leisure
- Former car park land required
- Proposed retail units
- Unaffected retail units
- Proposed entrance/exit to car park
- Proposed area of car parking to be reallocated

Rev	Date	Detail	Issue	Created	Approved
-----	------	--------	-------	---------	----------

Project: Bradford Place Bus Interchange

Title: Proposal Option 2 Jerome Retail Park Indicative Land Affected Plan

Design: RT	CAD: RT
CHK'd: MC	App'd: MC
Date: 18.04.17	Scale: NTS



No. Rev.