

4.2.2 Town End

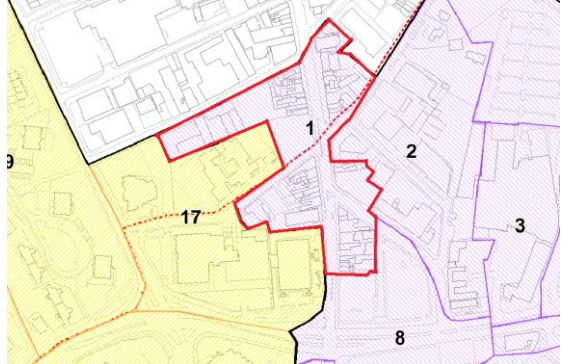



The Town End Character Area characterises much of the building eras of Walsall, covering some of the Victorian industrial precincts of Walsall, demonstrating the leather making and metal working heritage of the town; to the 20th Century clearance with a number of open areas of land; to the mid 20th Century developments such as the Ring Road and associated office buildings, to the more recent developments of large scale shopping complexes and educational Institutions. Along with this there are pockets of housing development from the Victorian terraces and associated workshops along Station Street, to the more modern housing developments and cul-de-sacs present in Saw Mill Close.


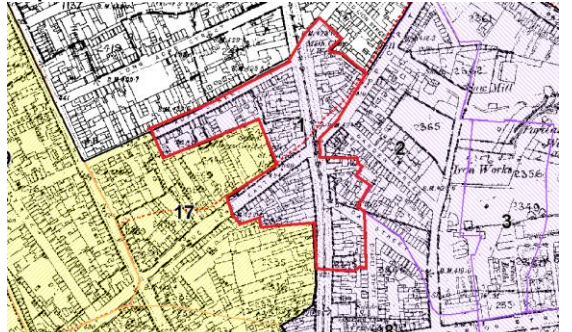
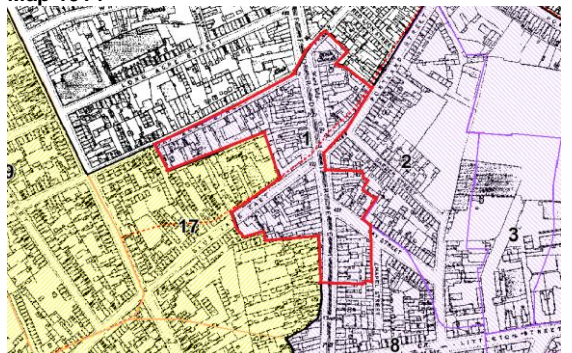
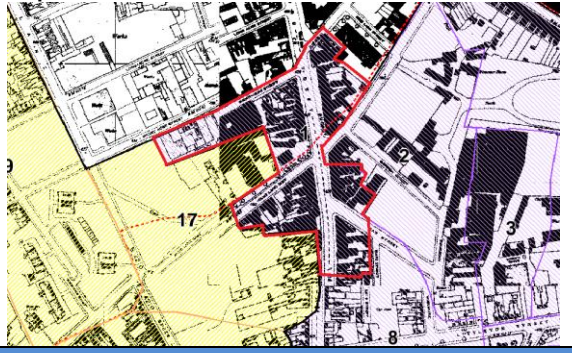
The area also demonstrates the history of transport in Walsall, with evidence of laneways and carriageway entrances for horse and cart, and use of the canal network with the wharf and wharf buildings still present. The railway and the move towards vehicular transport and connectivity with the formation of the ring road and the need for large car parking areas to cater for today's commuter demands are also present with in this area.

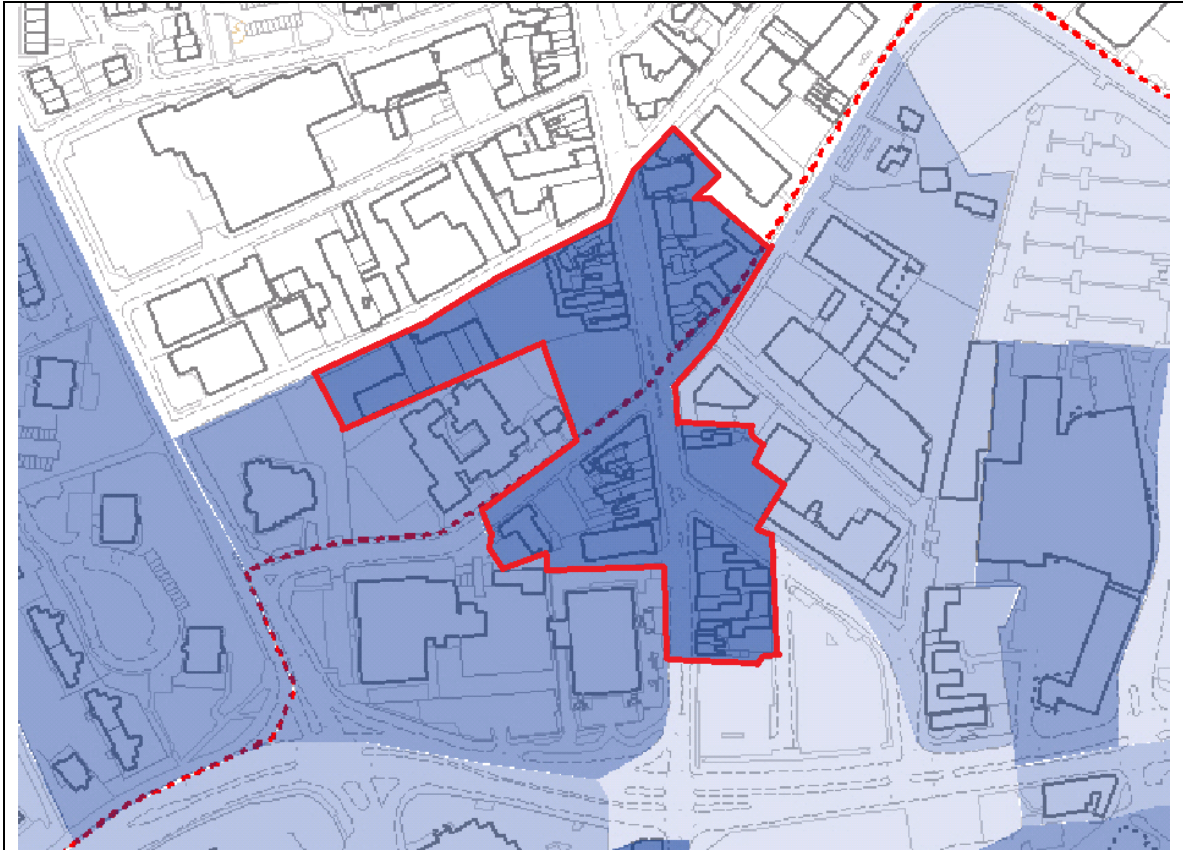
Building form and scale varies greatly throughout this area and is related to their era with a haphazard of building types and styles. Victorian buildings in the character area generally have no setback from the street and range in height from 2-3 storeys and typically of brick finish and construction. Although in some areas such as Station Street, the Victorian townscape is maintained, in other areas such as the Leather Museum and Wisemore House, the buildings stand isolated amongst modern office and retail facilities as well as the ring road and car parking facilities.

The more modern developments such as office buildings, art gallery and educational college are large buildings normally setback from the street frontage and have a much larger scale and height which act as focal points and landmarks throughout the Town. Design and construction are with modern building materials of concrete, typically clad and rendered along with large areas of car parking and the introduction of multi-storey car parking areas.

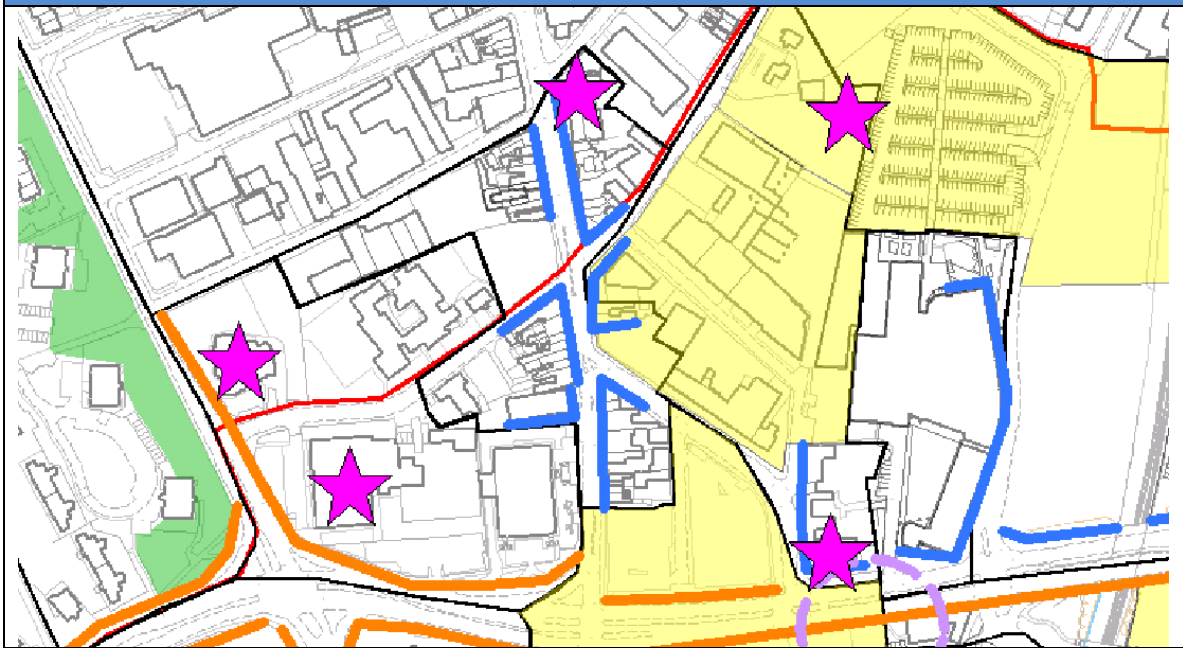
4.2.2.1 Stafford Street (North)

Character area: Town End - Stafford Street HUCA: RE2	Sub character area ref: TE1 Survey date: 9 th July 2014
<p>An established area of 19th century development along an historic route linking Walsall to Bloxwich. The historic plot arrangements are largely intact and a wealth of mid Victorian two or three-storey shops survive.</p> <p>The character area continues in this form northwards along Stafford Street, but the surrounding areas have been redeveloped throughout the 20th century and do not reflect this degree of completeness. Redevelopment of surrounding areas has been so complete that this area is very distinctive and appears as a hub of commercial activity.</p>	
<p>Building types:</p> <p>Buildings largely date from the 1850's to 1900 and predominantly comprise shops with accommodation over in the form of two or three-storey buildings sat at back of pavement with shop fronts with console and wedge lintel windows to the upper floors.</p> <p>Some warehousing exists in side streets, but on modest plots along with tanneries and other small industrial trades.</p> <p>A non-conformist chapel (now a temple) is a contemporary of the wider townscape and uses the broad classical architectural language used throughout the area, incorporates pediments to elevate its status.</p>	
<p>Connections and urban grain:</p> <p>Buildings are all sat at back of pavement and form strong street corridors in contrast to adjacent areas, where this deteriorates abruptly into open 20th century planning.</p> <p>Buildings are generally small and terraced so that the townscape is of a tight urban grain.</p> <p>All buildings front directly onto the street and there are few blind frontages. There are few open spaces and the character of the area is intimate and tight knit.</p>	
<p>Uses:</p> <p>The majority of uses are related to commercial activities including shops, light industry and trade counters. Fast food outlets and cafes also exist. As is typical with older established areas there are also a mix of higher order buildings including educational buildings and places of worship. This continues on outside of the character area north along Stafford Street.</p> <p>Industry is more prevalent along side streets, but is largely concerned with storage and light industrial ventures.</p>	

<p>Predominant materials:</p> <p>Brick dressed in stone is the predominant building material throughout the area and roofing is typically Welsh slate.</p> <p>Over the course of the 20th century buildings have been rendered and brickwork has been painted.</p> <p>Joinery details are sparse but constitute shop fronts and windows, although again these have frequently been replaced by modern materials such as UPVC plastic and powder coated aluminium.</p>	
<p>Positive characteristics:</p> <p>The survival and a significant proportion of original 19th century buildings allowing for the rhythm of the street to be retained through the repetitive sequence of small plots, either through terraced arrangement or sequence of individual structures.</p> <p>The consistency of the townscape through the construction of a single building type, punctuated by occasional higher order buildings or either greater scale or architectural treatment.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The loss of some buildings damaging the completeness of the townscape and the use of these sites for surface level parking and servicing</p> <p>The deterioration of the areas condition through poor building maintenance, poor alterations and adaptations, poor changes of use, gaudy paint finishes and excessive use of poor quality signage and security measures.</p> <p>A degree of vacancy exists in the area and is of a long term and derelict nature.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>An area of highly typical 19th century development in the Black Country where an intimate mix of uses coexisted. Plots housing shops with accommodation over and light industrial activity in rear yards.</p> <p>The architectural detailing of buildings is also highly localised and represents the evolution of terrace buildings across the Black country with the elevated eaves, wide spacing of windows and window lintels supported by consoles.</p>	<p>Map 1977</p> 
<p>Sensitivity: 2</p> <p>The area retains its historic layout and much of its 19th century development and therefore is relatively sensitive, however, as buildings are in poor condition and have been insensitively altered the sensitivity is lessened to some degree.</p>	

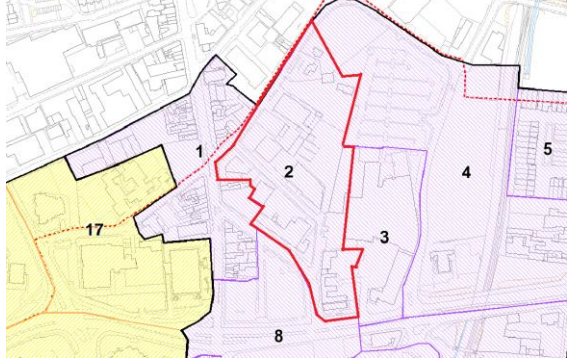






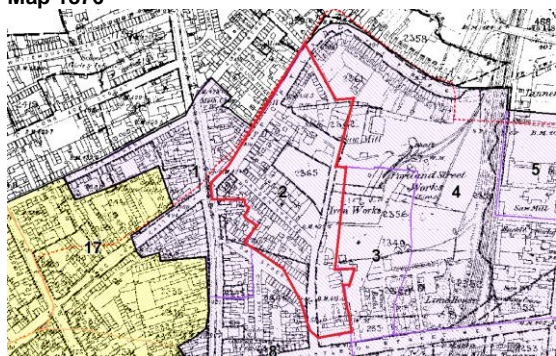
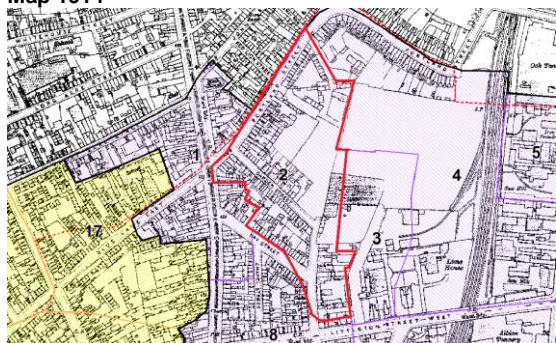

Capacity for change: There is limited capacity for change in this area, other than the incremental alterations and redevelopment of sites, with a general presumption to retain buildings and built scale and form.

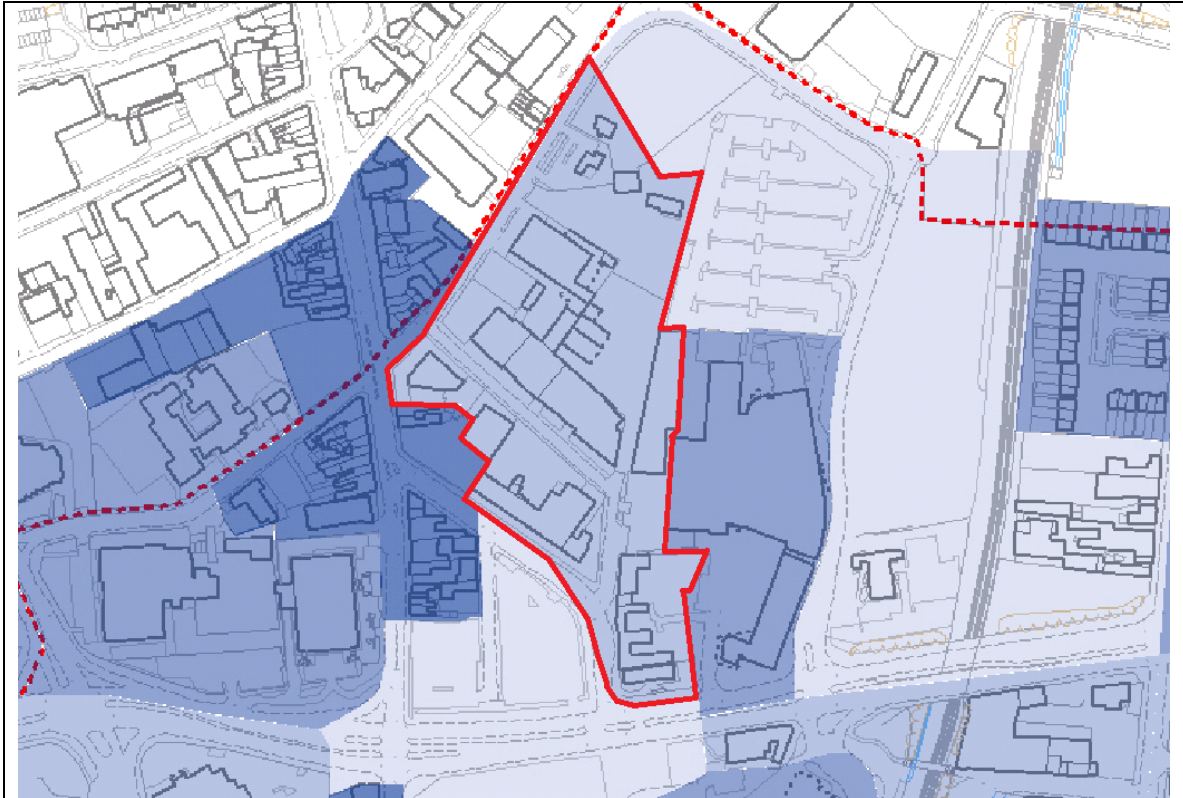


Opportunities: There are opportunities to strengthen existing landmarks, such as the temple and develop the limited vacant sites (including that along the western side of Stafford Street north of the Blue Lane East junction and along the eastern side of Stafford Street north of the Day Street junction).

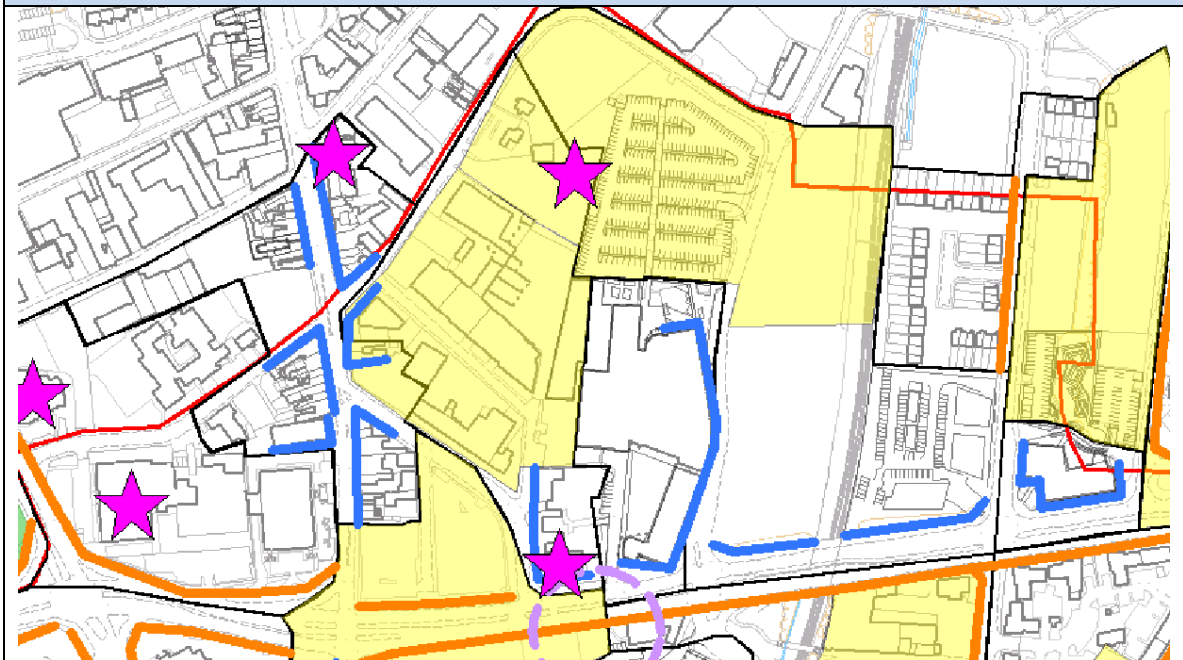
4.2.2.2 Wisemore

<p>Character area: Town End - Wisemore</p> <p>HUCA: IN4</p>	<p>Sub character area ref: TE2</p> <p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>An area of industrial development on the edge of the 19th century town, where 20th century clearance and redevelopment has resulted in a mix of building types arranged haphazardly on large plots of land.</p> <p>There is a similarity with the area to the north, and some affinity between the areas older buildings and sections of Stafford Street to the west. Land to the south and east has been cleared and in part redeveloped and therefore there is a stark contrast between this area and land in these directions.</p>	
<p>Building types:</p> <p>The leather museum and Wisemore House are the only surviving historic buildings in the area. The museum is a typical example of 19th century utilitarian workshop construction of evenly spaced segmental windows over a plain façades. Wisemore House is fine detached house based on a turn of the 19th century model of a villa, only with workshops attached.</p> <p>Most other buildings date between the 1950's and the 1980's and comprise traditional built offices and modern warehouse sheds.</p>	
<p>Connections and urban grain:</p> <p>Other than the Leather Museum, the majority of buildings are divorced from the street, either being set back behind forecourts, set within aprons of hard standing or facing into internal cleared sites. Factor in cleared and vacant sites and the area presents no visible form of management or planning.</p> <p>The density of development is exceptionally low for a town centre location and with the ad hoc arrangement of parcels of land, sits frequently expose their servicing arrangements and back of house functions.</p>	
<p>Uses:</p> <p>The Leather Museum is located at the southern end of the area, on Day Street closest to the ring-road, and is compatible with its proximity to the town centre proper.</p> <p>Further up Day Street are retail units, hire shops, trade counters and showrooms. Beyond this are larger sheds and commercial premises that accommodate light industry and associated offices.</p> <p>Vacancy is a significant issue in the area which sits in conjunction with dereliction and surface car parking.</p>	

<p>Predominant materials:</p> <p>Traditional buildings are constructed in brick or are rendered (the museum and Wisemore House respectively). Welsh slates roof these buildings.</p> <p>The wider pallet of materials across the area comprise sand faced brick, brown brick, exposed concrete framing, and a variety of cladding systems. Most cladding options are utilitarian and functional and comprise a ribbed or corrugated finish. The roof of these buildings are largely not visible.</p>	
<p>Positive characteristics:</p> <p>The museum is an excellent example of Black Country workshop construction and is located in a prominent location affording good townscape merit.</p> <p>Wisemore house as a listed building is a significant piece of architecture within the area and comprises a hansom building of significant interest.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The unplanned arrangement of buildings along with the degree of open space, servicing, hard standing, car parking and wasteland results in a disparate townscape of piecemeal construction. Boundaries are marked by fencing which adds a further layer of ad hoc management to the pattern of development.</p> <p>The condition and appearance of many buildings is of poor quality and utilitarian affordability. There is no prevailing building material or design and as such the area is a collection of unkempt sheds and units.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>The area contains some exceptional examples of domestic and commercial buildings from the 19th century's industrial period.</p>	<p>Map 1977</p> 
<p>Sensitivity: 4</p> <p>The area contains two important historic buildings (1) Wisemore House (Grade II listed building), and (2) the Leather Museum (traditional industrial works), other than these the area comprises poor quality building, most of which are set back from the street behind parking aprons and open yards and not contribute positively to the area.</p>	

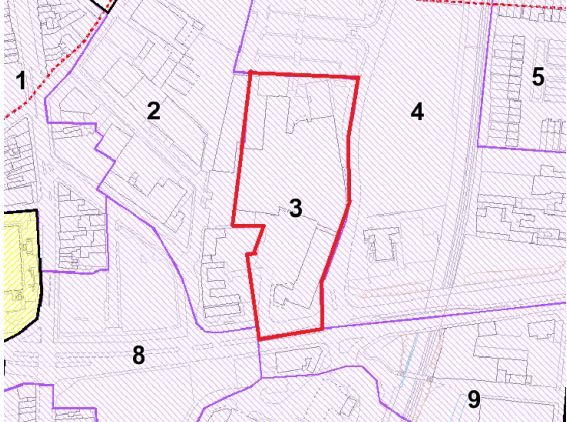







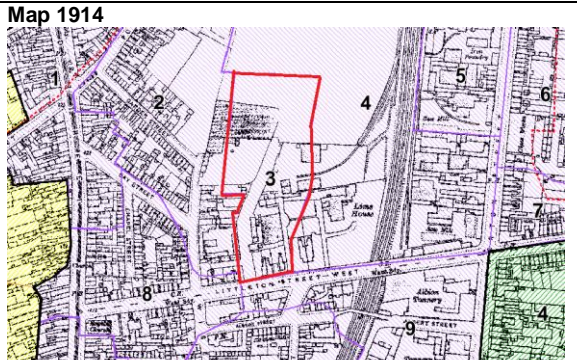

Capacity for change: There is significant capacity for change subject to finding ways of retaining the alignment of Wisemore Street and finding ways to reconnect Wisemore House back to Portland Street. Other than Wisemore House and the Leather museum, there is substantial capacity for comprehensive change.

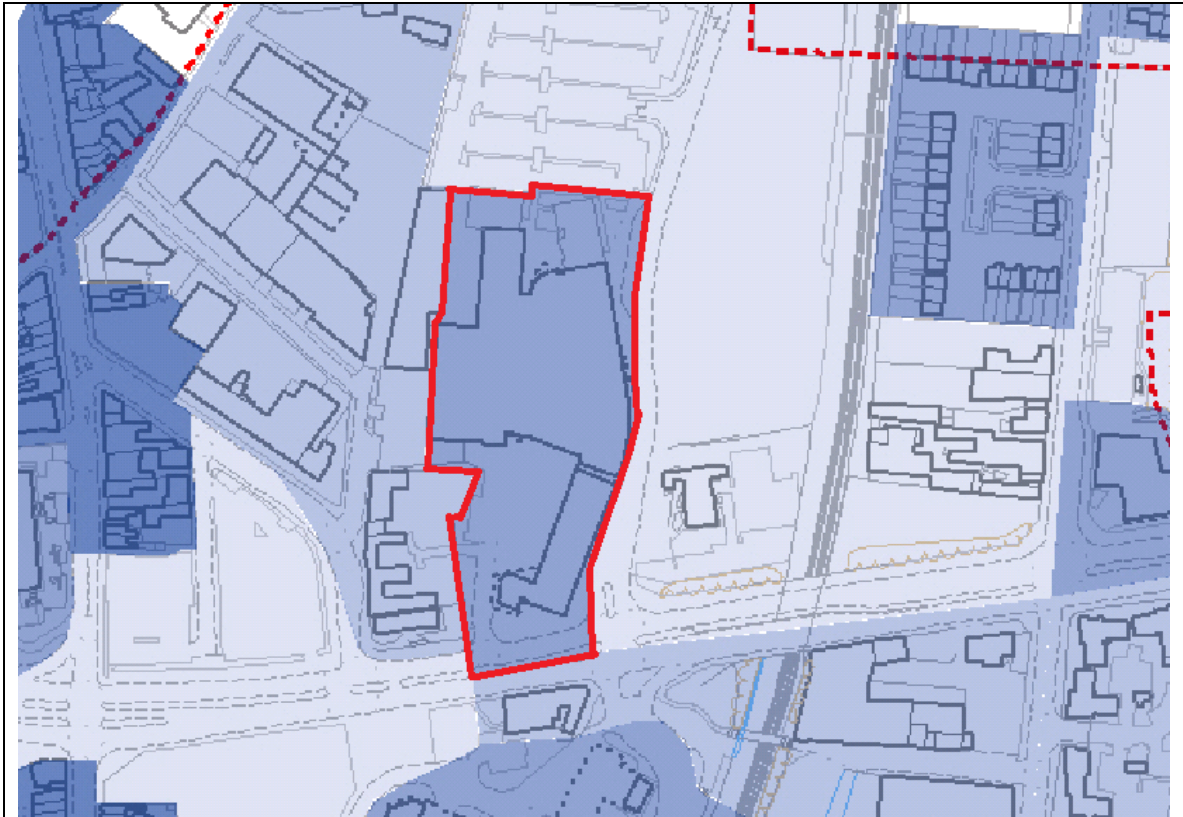


Opportunities: The majority of the site presents significant opportunities with numerous options as to how the area can be redeveloped. Setting of the listed building should structure any masterplanning exercise and ensure the building is reconnected back to the existing highway network and wider townscape. Development up to 4 storeys in height should be considered. Any opportunities for improving connectivity across the ring road should be secured.

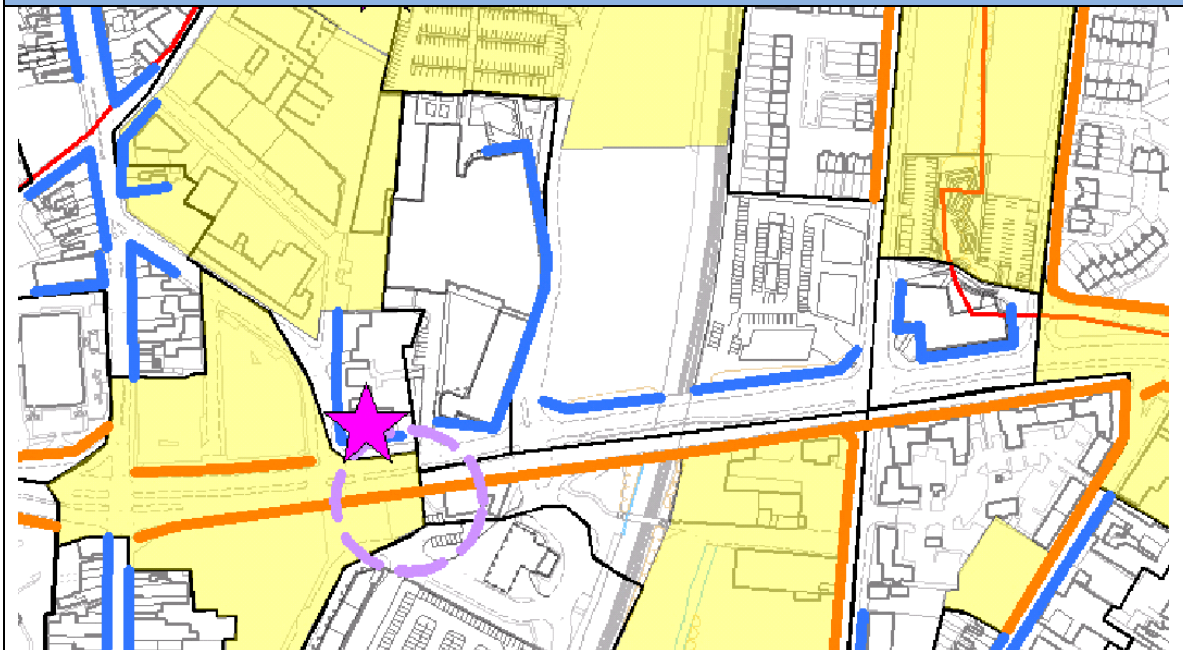
4.2.2.3 The College

<p>Character area: Town End – The College</p>	<p>Sub character area ref: TE3</p>
<p>HUCA: C4</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>A new purpose built educational building fronting directly onto the towns northern stretch of the ring road. A building of significant scale and confident architecture that contrasts with the towns historic industrial architecture that surrounds it, including the works to the west (now the Leather Museum) and the clerks offices on the south side of the ring road.</p> <p>The land to the north and the east comprises cleared land which extends beyond this area where the college has been built.</p>	
<p>Building types:</p> <p>A single large scale building, likely to be one of the largest in the town. The building is five-storeys in height and comprises a number of substantial blocks of massing, unified into one form. The eastern aspect is consolidated into a long convex curve towards the street.</p> <p>Groups of windows are punched through the cladding in an irregular format and contrast with large areas of solid cladding.</p> <p>There is in part a similarity in aesthetics with the New Art Gallery.</p>	
<p>Connections and urban grain:</p> <p>The building takes an abstract form on its plot and comprises open landscaping (similar to the Hatherton Street Building, further along the ring road to the east) that merges with the public realm and highways.</p> <p>The scale of the building is confident and accepts a new scale of plot formation in the area of the town centre. No reference is made to an earlier scale of development and this reflects the loss of all structures to the east.</p>	
<p>Uses:</p> <p>Entirely used for further educational purposes.</p>	

<p>Predominant materials:</p> <p>The building uses materials that focus on a pallet of complimentary greys. The key material towards the ring road is vertically applied standing seam metal cladding, adjacent to this is blue brick, white and grey rendering, structural glass and spandrel panels.</p>	
<p>Positive characteristics:</p> <p>The building is a good example of modern architecture that is both confident and assertive and addresses the ring road boldly.</p> <p>The building sets the precedent for the scale and type of built form along the ring road and forms part of an early phase to enclose the ring road.</p> <p>The building is a significant generator of activity along the northern stretch of the ring road and will facilitate future regeneration in this area.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The building lacks activity at ground floor and has significant stretches of blank frontages towards the ring road.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>A prominent site along the northern stretch of ring road that signposts the scale of new development along the northern edge of the town centre.</p>	<p>Map 1977</p> 
<p>Sensitivity: 3</p> <p>The area is fully developed with a modern building of bold confident design. The structure is visible from strategic vantage points along the ring road and across the town and therefore is sensitive, albeit not precious.</p>	

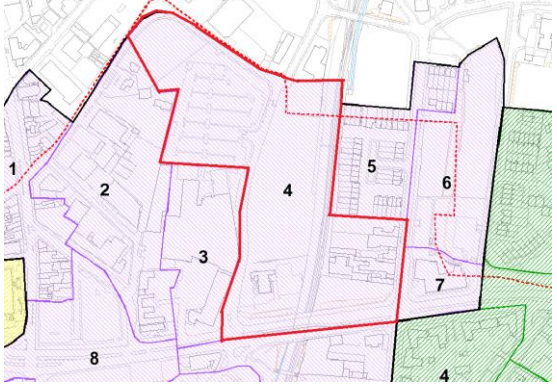






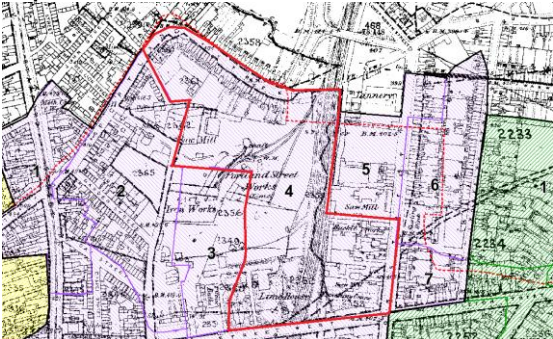
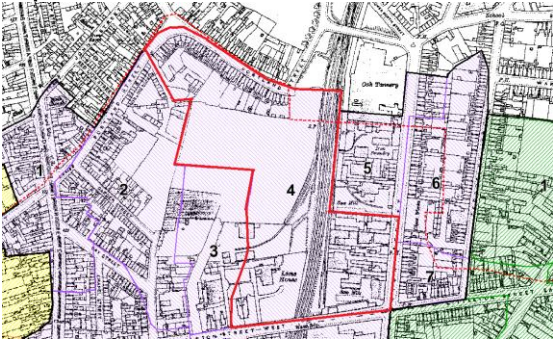

Capacity for change: There would be no resistance to undertaking changes to this site subject to design quality. The scale of the building is as high as could be tolerated in this location.

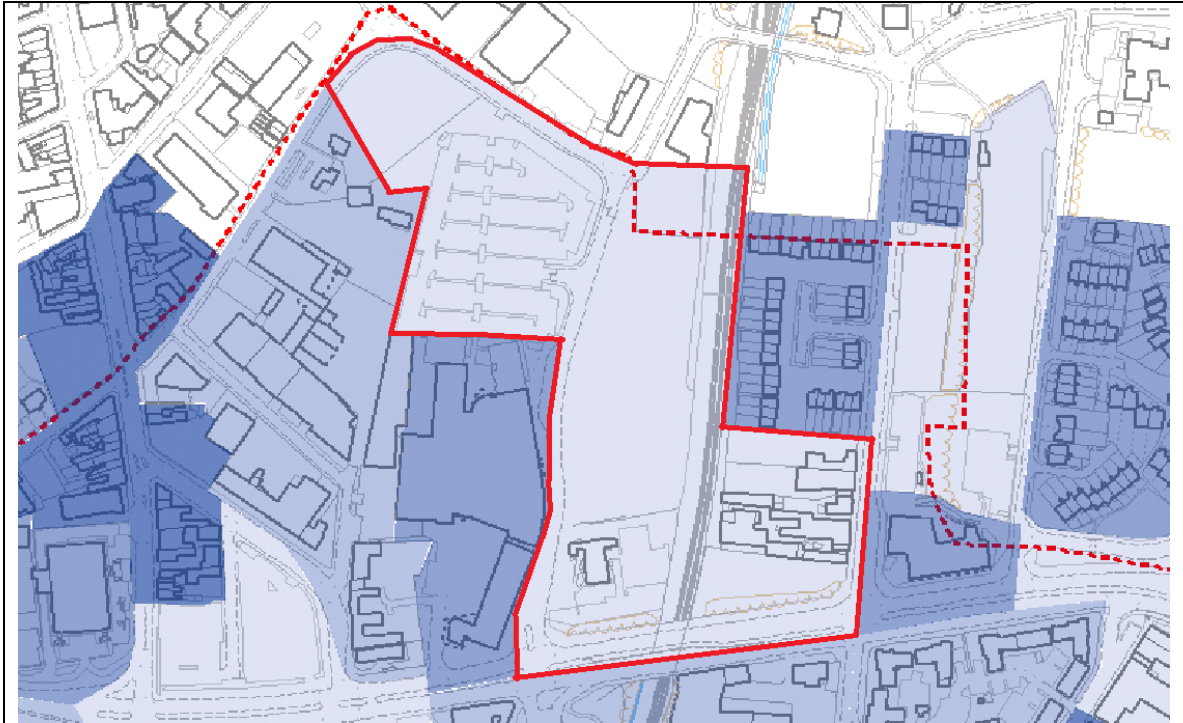


Opportunities: There are no current opportunities in this area, although any opportunities for improving connectivity across the ring road should be secured.

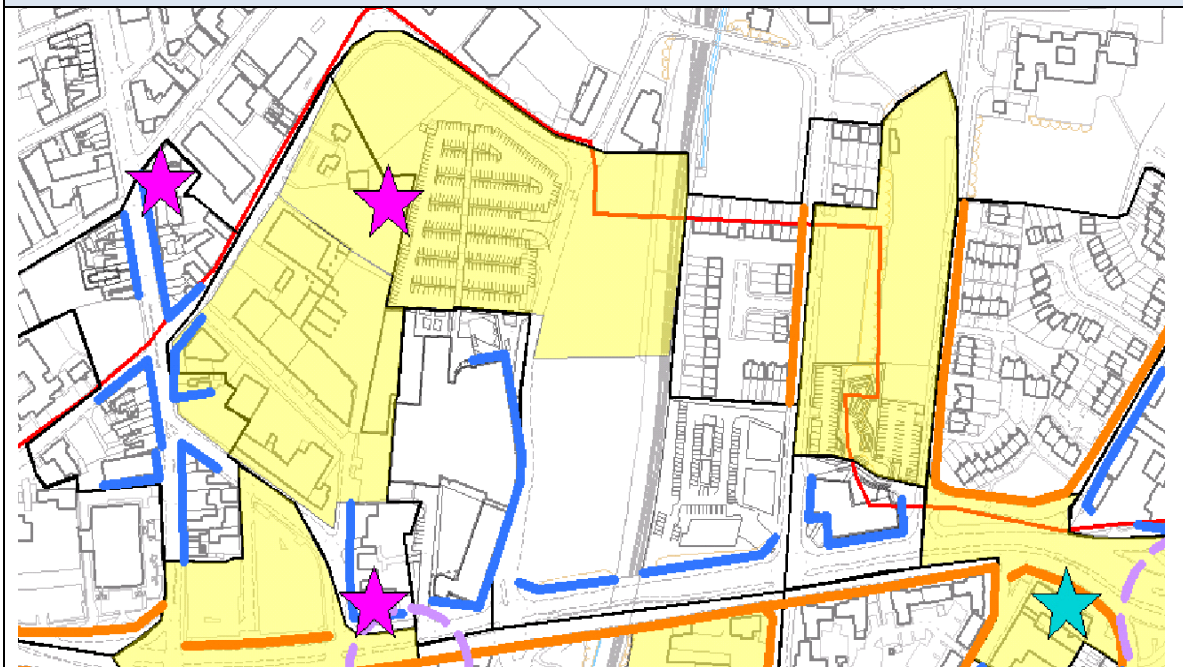
4.2.2.4 Portland Street

<p>Character area: Town End – Portland Street</p>	<p>Sub character area ref: TE4</p>
<p>HUCA: C4</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>The site comprises a significant frontage onto the northern stretch of the ring road and has recently been cleared of buildings. It is set down from the ring road (to the south), but is at grade with Hatherton Street (to the east) and elevated above the railway line (that runs through the centre of the area). The area extends back to the rear of the college site, wrapping round the modern housing in Saw Mill Close.</p> <p>The front part of the site has been remediated and development is being constructed for new commercial office buildings at four-storeys in height, whilst the majority of the site is undeveloped, except for a surface car park to the rear of the college.</p>	
<p>Building types:</p> <p>No buildings currently exist, although a building is being constructed on the ring road frontage. It is an office building that comprises a deep base (ground floor) that elevates the active floors above street level. The building relates in scale and architectural aesthetics to 100 Hatherton Street (to the east) and the wider masterplan for this stretch of the ring road. The elevation employs a strong modernist horizontal expression across each floor through the linear arrangement of windows inter-dispersed with a panelling system and the plan is set on a parallelogram so as to exaggerate the buildings perspectives.</p>	
<p>Connections and urban grain:</p> <p>The site is currently divorced from the ring road through lowered topography which the building under construction will address through open hard landscaped at the corner with Hatherton Street to mirror that on the opposite corner and create a clear connection with this existing piece of urban public realm.</p> <p>Albert Street runs parallel with the railway line and also transects the site, but the lack of buildings both within the site and in the inner suburbs to the north, result in an urban grain that has been entirely lost.</p>	
<p>Uses:</p> <p>Other than the surface level car park and office building under construction, there are no uses in this area, although this does fall under the 'Gigaport' masterplan which seeks a mixed use development in this area.</p>	

<p>Predominant materials:</p> <p>The building under construction will use a combination of rain screens claddings that will reflect the pallet of materials that have been used on 100 Hatherton Street and the college building.</p>	
<p>Positive characteristics:</p> <p>The implementation of the commercial building on the ring road frontage will start to reinstate enclosure of the site and the ring road itself, which in conjunction with 100 Hatherton AStreet and the college is starting to complete this frontage along this key artery into the town.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The loss of buildings and structures across an extensive swath of land which now largely plays host to self seeded trees and scrub land rather than development to replace what has been lost.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>A frontage along a key arterial route forming part of the towns ring road. A critical central section of a major masterplan to secure modern commercial floorspace that will support the towns economic regeneration.</p>	<p>Map 1977</p> 
<p>Sensitivity: 5</p> <p>The area is devoid of building other than those being constructed along the ring road frontage. The urban grain has also disappeared and therefore the site has little sensitivity.</p>	




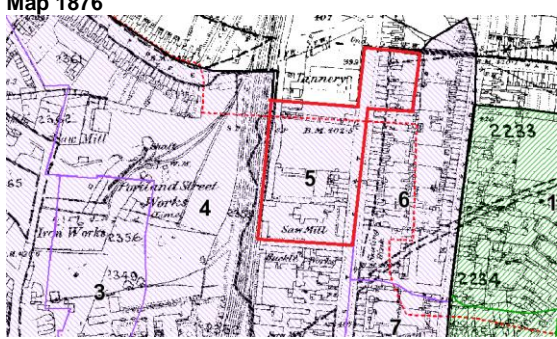
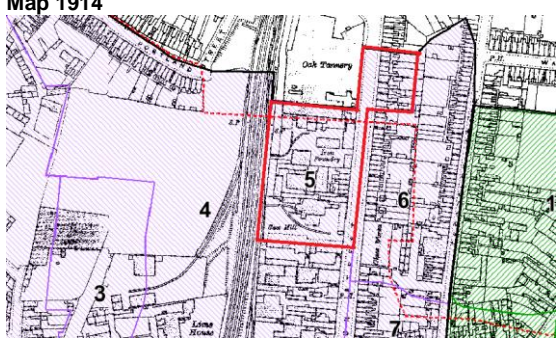
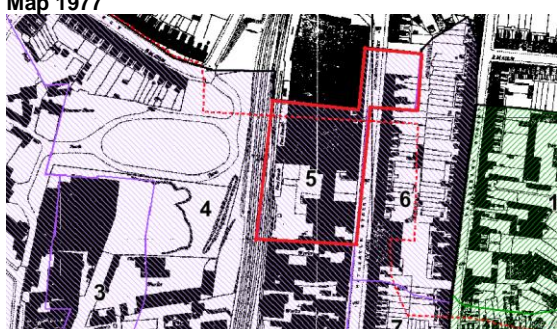
Capacity for change: There is significant capacity for change including the construction of sizable buildings (up to 6 floors in height, with design mitigation for the amenity of neighbouring properties). This includes possible future height increases and design modifications to the buildings under construction along the ring road frontage.

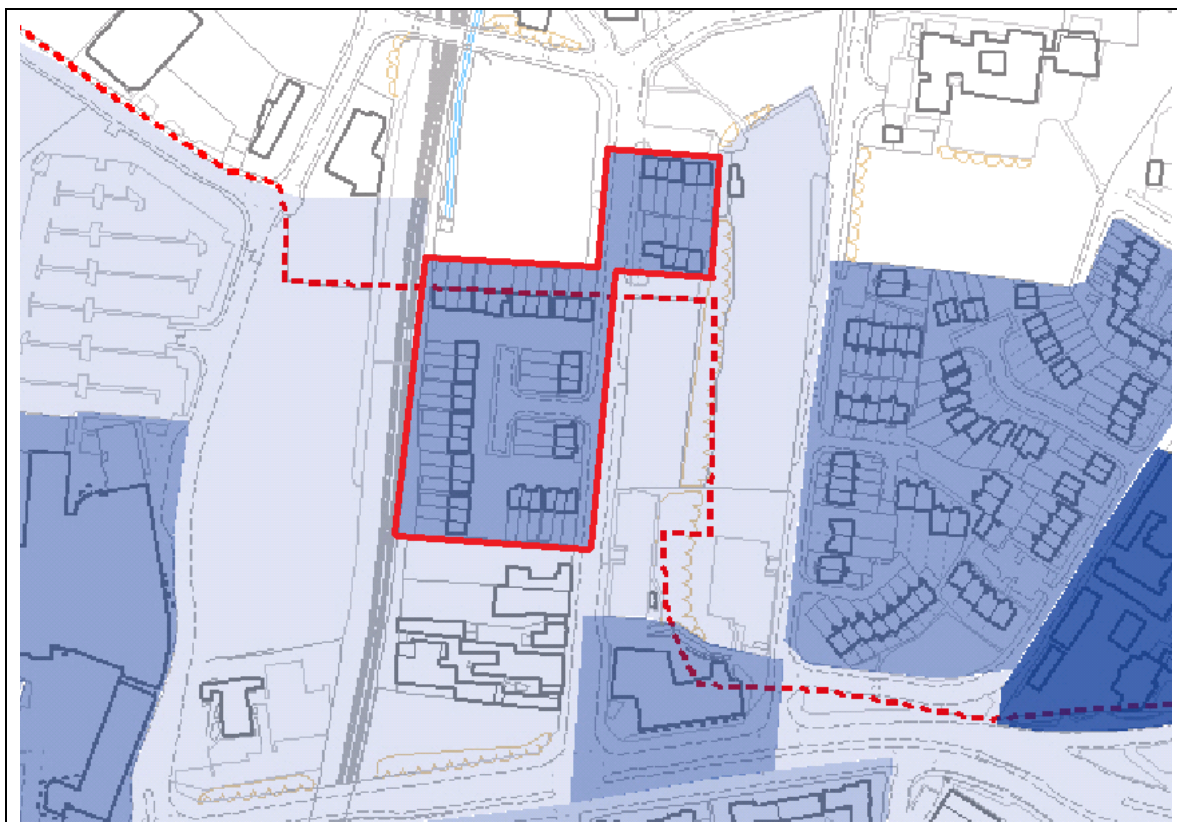


Opportunities: The site presents substantial opportunities for comprehensive redevelopment in connection with the Wisemore area to the west.

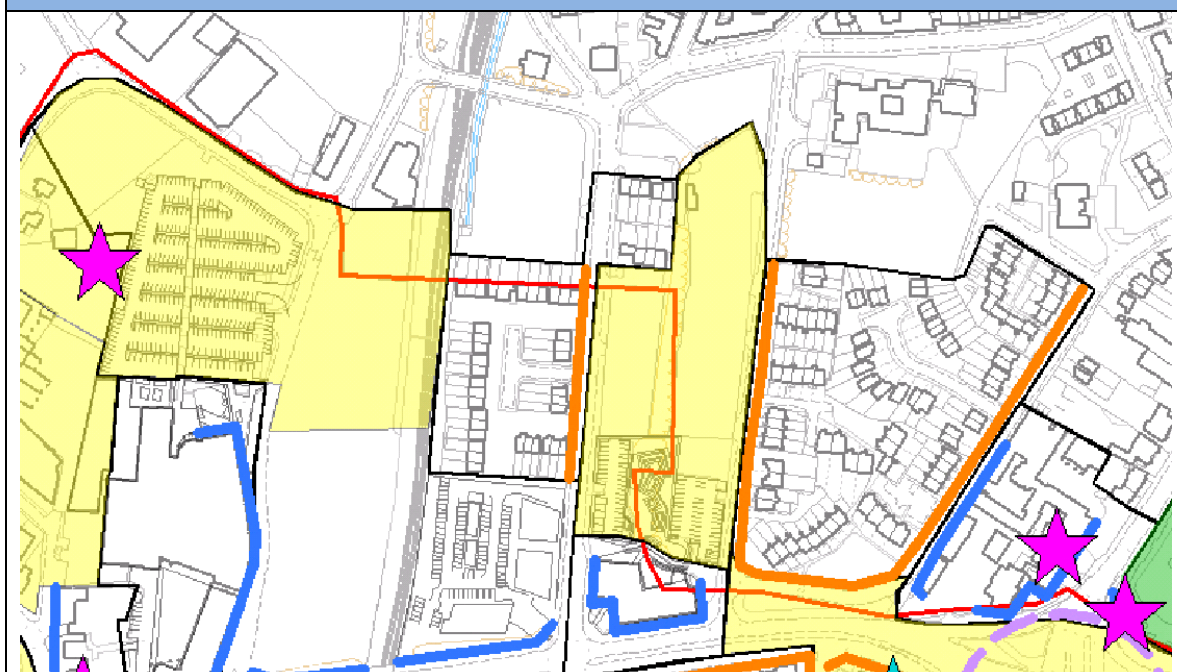
4.2.2.5 Saw Mill Close

<p>Character area: Town End – Saw Mill Close</p>	<p>Sub character area ref: TE5</p>
<p>HUCA: RS8</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>A section of windfall housing development that employs a standard housing layout for dense modern terrace housing.</p> <p>There is very limited relationship with surrounding areas, with only a few properties facing onto Hatherton Street itself, with the majority facing into individual cul-de-sacs and presenting blank flanking gables to the street.</p> <p>Surrounding areas comprise development sites that form part of a masterplan for commercial uses which will not relate to this area in scale, design, use or connectivity.</p>	
<p>Building types:</p> <p>Semi-detached and terraces of modest modern housing in short section of three units.</p> <p>The buildings are simple and comprise little more than pitched roofs on orthogonal shells. The only architectural expression are a series of mono-pitched porch canopies and string course separating ground and first floor.</p> <p>Ridges and associated eaves, along with front elevations are subtly stepped to break the form of a conventional terrace.</p>	
<p>Connections and urban grain:</p> <p>A series of short cul-du-sacs all leading off of a single access point onto Hatherton Street. Where street lead round and back towards Hatherton Street, they are separated off by walling or railings.</p> <p>Parking courts are a dominant feature and include wide strips in front of housing. Front gardens are modest and either open to parking courts or surrounded by low hoop-topped railings.</p> <p>Some properties front the backs of others and this has resulted in the prominence of garden fences in the street scene.</p>	
<p>Uses:</p> <p>The entire are is residential.</p>	

<p>Predominant materials:</p> <p>Red or buff brick with the alternative brick uses as a feature brick in banding and as a gable feature.</p> <p>Grey concrete roof tiles and brown or white joinery and green metal railings.</p>	
<p>Positive characteristics:</p> <p>A short stretch of housing fronting onto Hatherton Street.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>A number of blank gable elevations fronting onto Hatherton Street itself, which in conjunction with sections of railings and walling to fill in the street frontage, close down animation and surveillance.</p> <p>Very poor layout where fronts of buildings facing extensive parking courts and rear fencing lines.</p> <p>Very poor architecture comprising buildings with applied elements of basic design (through materials) and wavering of roof heights and building lines resulting in an unconfident grouping of units.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>An area of modern housing development that predates the masterplanning of the area.</p>	<p>Map 1977</p> 
<p>Sensitivity: 3</p> <p>The area comprises modern housing of no particular character, it reflects the scale of traditional inner suburban residential areas, but lacks the density and design of such areas. It is not acceptable development, but not sensitive.</p>	

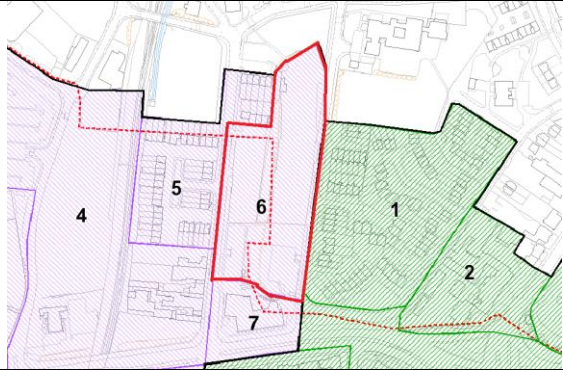






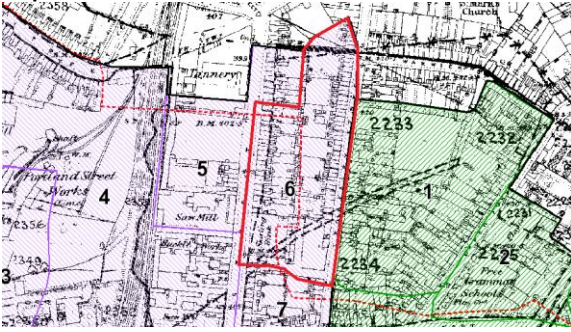
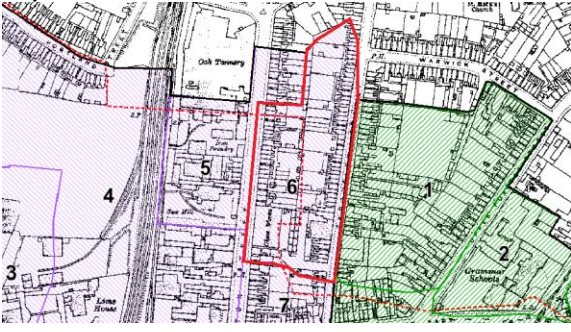
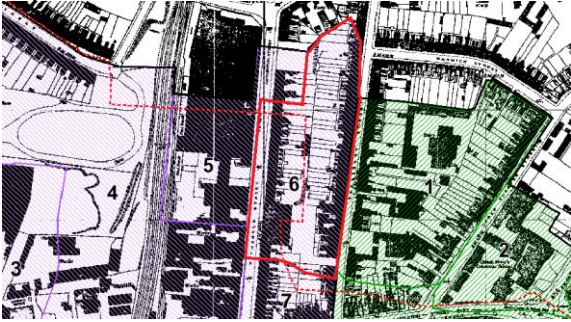
Capacity for change: There is capacity to change the character and design of this area, including scale and layout.

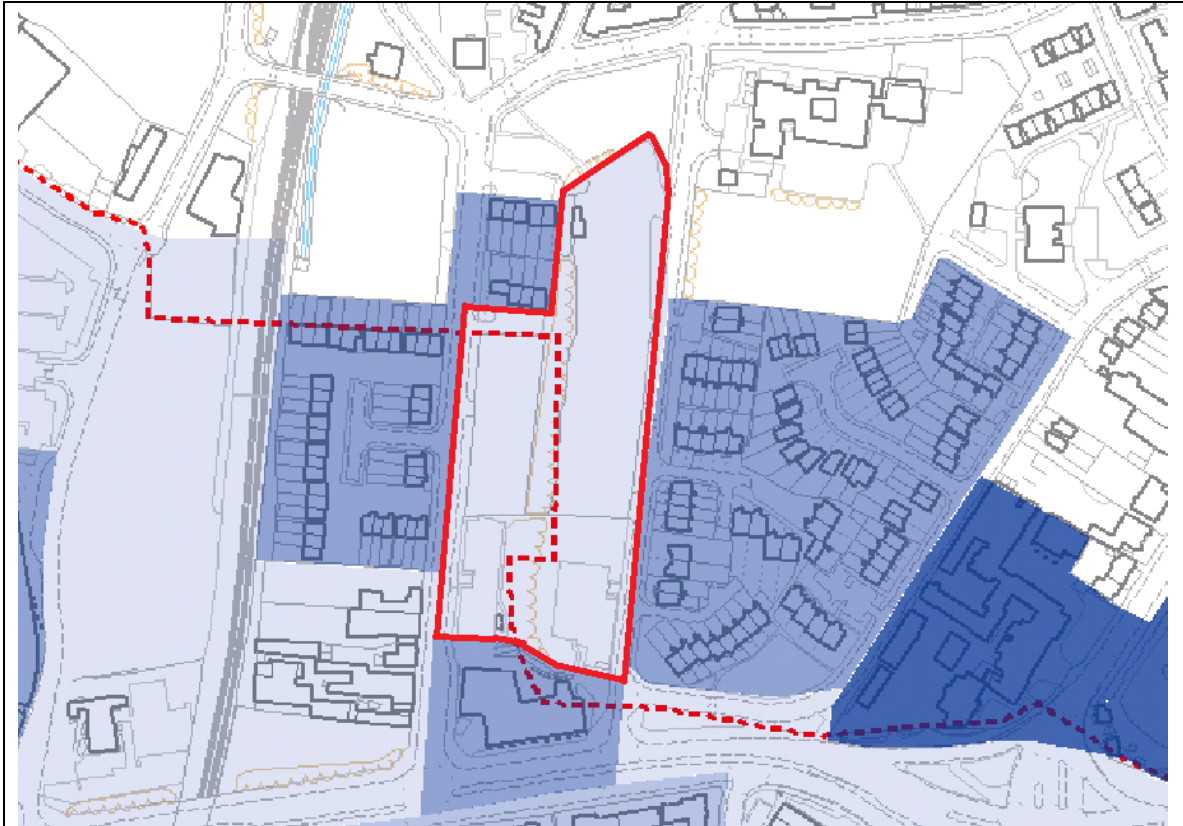


Opportunities: There is little opportunity for change due to the high level of fragmented ownership.

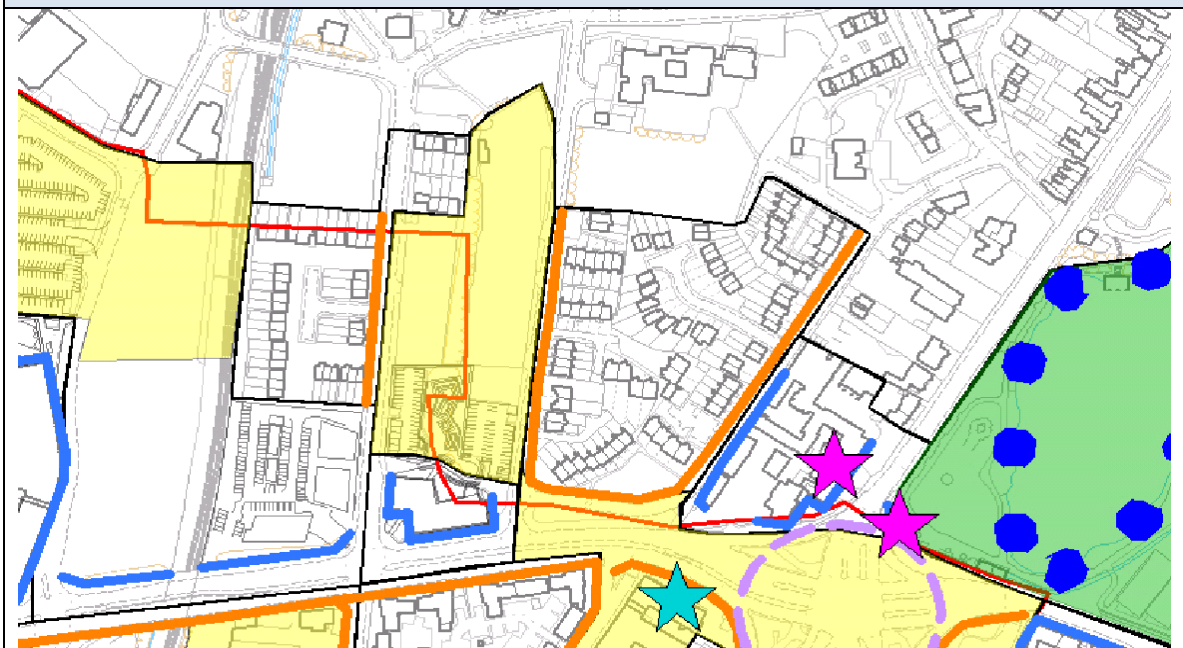
4.2.2.6 Hatherton Street (North)

<p>Character area: Town End – Hatherton Street (North)</p>	<p>Sub character area ref: TE6</p>
<p>HUCA: OP3</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>An extensive area of surface level parking, delivered in two tranches.</p> <p>The site is screened from the ring road by 100 Hatheron Street (that fronts the ring road) and for which one of the car parks exclusively serves.</p> <p>The sites does not otherwise relate to the surrounding areas of modern housing or redevelopment in any way.</p>	
<p>Building types:</p> <p>There are no buildings.</p>	
<p>Connections and urban grain:</p> <p>The car parks are served by single access points and therefore do not connect to the surrounding network of streets or provide a series of development plots that create a rhythm along the surrounding Streets.</p> <p>The sites are well matured with trees and soft landscaping.</p> <p>The southern car park flows into the public realm of 100 Hatherton Street and the fencing that secures the northern (public) car park is removed entirely to allow for better pedestrian access.</p>	
<p>Uses:</p> <p>Car parking.</p>	

<p>Predominant materials:</p> <p>Tarmac and soft landscaping.</p>	
<p>Positive characteristics:</p> <p>The extensive soft landscaping which includes banking to the eastern margin provides a pleasant green space that screens the parking from the housing to the east. The trees along the boundary to Hatherton Street offer significant mitigation for this use along the western aspect.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The use of such a large area for parking alone impacts on Hatherton Street, creating a dead frontage with limited surveillance and poor levels of activity.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>An area of car parking that predates the masterplanning of the area, but supports future growth.</p>	<p>Map 1977</p> 
<p>Sensitivity: 5</p> <p>The area is devoid of buildings or any reference to urban grain and is not sensitive to change.</p>	

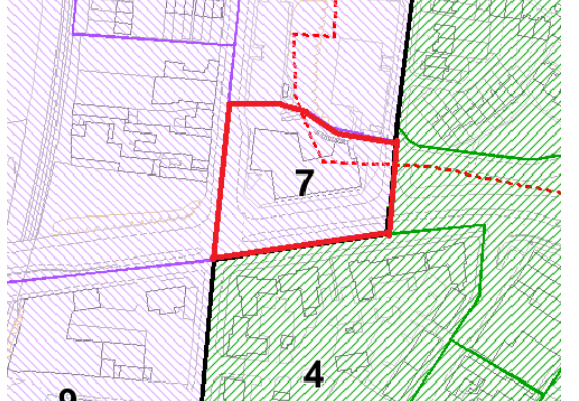






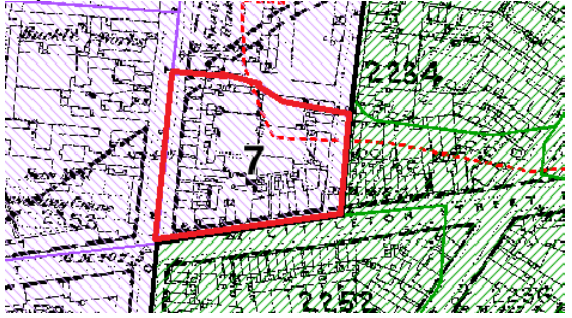
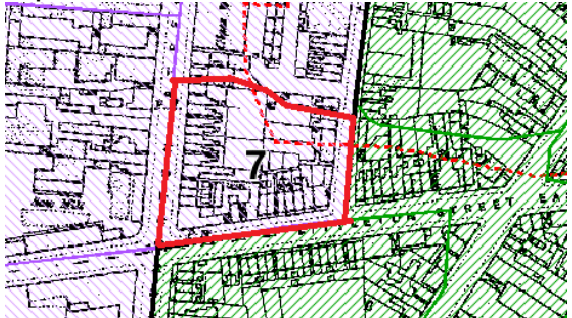

Capacity for change: There is a high capacity of change for this area including development of up to 4 storeys in height, subject to the preservation of amenity of surrounding dwellings.

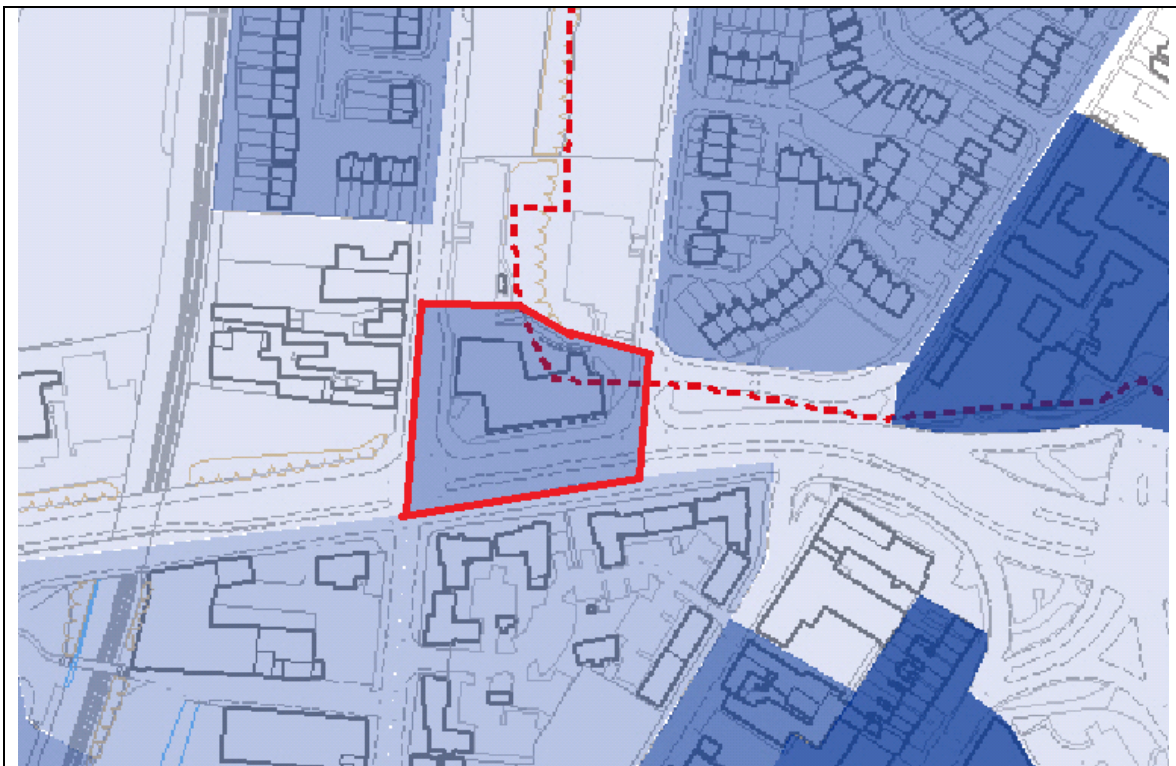


Opportunities: There is the possibility of a single development across the entire area, including over the existing car parking in connection with 100 Hatherton Street (stilted or otherwise).

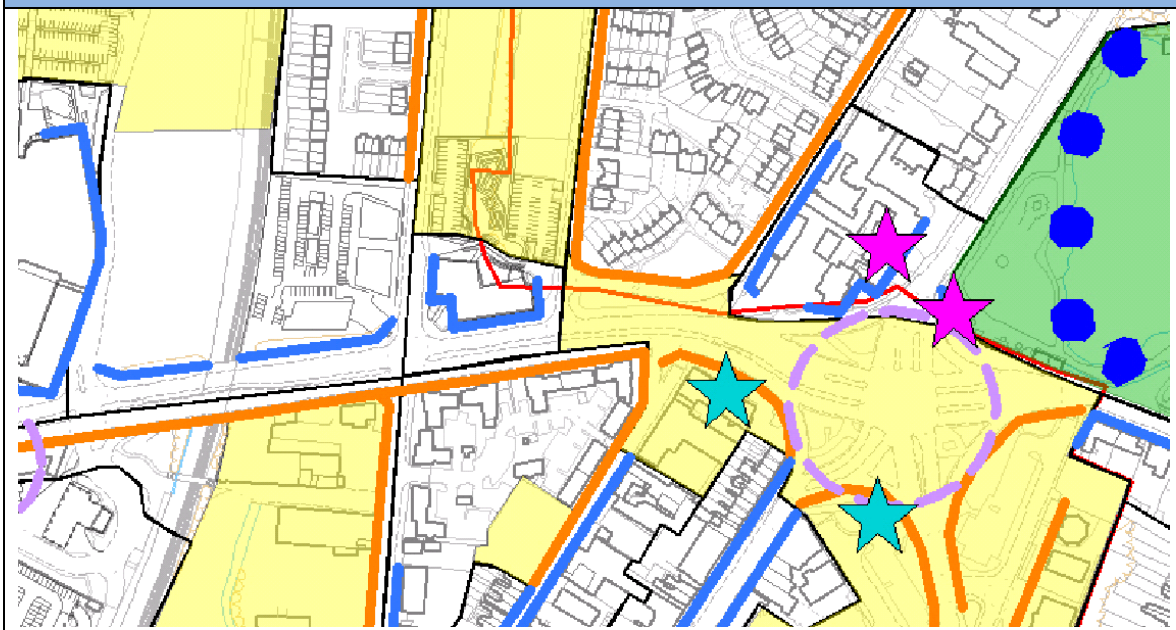
4.2.2.7 100 Hatherton Street

<p>Character area: Town End – 100 Hatherton Street</p>	<p>Sub character area ref: TE7</p>
<p>HUCA: C4</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>A sizable modern office building over four-storeys that sits along the northern side of this northern stretch of ring road around the town. It marks the corner of Hatherton Street and encloses the main thoroughfare in conjunction with the residential development on the southern side of the street.</p> <p>The buildings sheer use of design, innovative ground floor composition, abstract floor plate and position on the plot make it as one of the more successful new developments within the town centre.</p>	
<p>Building types:</p> <p>The building uses its steel frame to maximum architectural freedom. Entire elevations are delivered confidently using a single material, such as curtain glazing across the front elevation, meeting render on the side elevation at a corner than becomes a sharp arris at the meeting of these two surface finishes.</p> <p>Abstract etching into the curtain gazing reflects the angular plan of the building and the zigzag piloti that anchor the building into the ground.</p>	
<p>Connections and urban grain:</p> <p>The building is a single structure within its landscape. The building has a conventional parallel relationship to the ring road, but is angled back on the Hatherton Street corner to form a small area of hard-standing to address the entrance.</p> <p>To the rear, an imaginative use of terracing, decking and soft landscaping has been employed provide 'break-out space' and informal social spaces, that are set down into the rising topography, separating it from the car parking beyond.</p>	
<p>Uses:</p> <p>Commercial office space.</p>	

<p>Predominant materials:</p> <p>An innovative use of curtain wall glazing is the main material in the building and is used not only as a architectural expression that makes this building individual, but effectively screens (and works with) exposed floor plates. Coloured, tinted and etched/vinyl designs are applied differently on different elevations.</p> <p>The main material used to envelope areas of masonry is white render with complimentary areas of controlled fritted panelling to align with glazed areas.</p>	
<p>Positive characteristics:</p> <p>A building of exceptional modern design that comprises dynamic architectural form with a unique personality. The activity of the building is visible and provides substantial activity out onto the streets and surrounding courtyard areas.</p> <p>Whilst large, the building is of a human scale with spaces, enclosed by the buildings form and enveloped by its landscape.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The building has an unresolved relationship with the land to the east, comprising open landscaping between the ring road and the housing estate set back behind the Queen Mary's School.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>The building forms a dramatic start to the main section of the northern stretch of the towns ring road at the rise of the hill from the Arboretum. The building as such is a gateway from the east to this principal section of road.</p>	<p>Map 1977</p> 
<p>Sensitivity: 3</p> <p>The area is fully developed with a modern building of bold confident design. The structure is visible from strategic vantage points along the ring road and across the town and therefore is sensitive, albeit not precious.</p>	

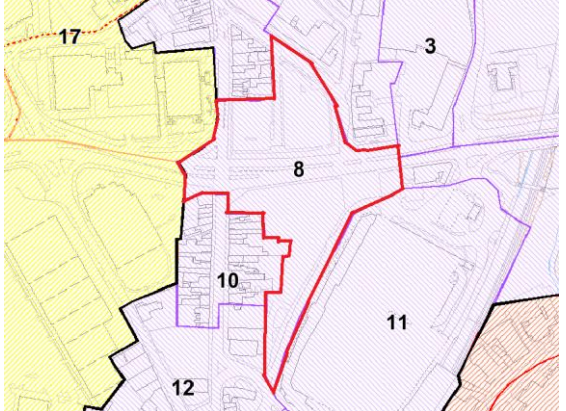






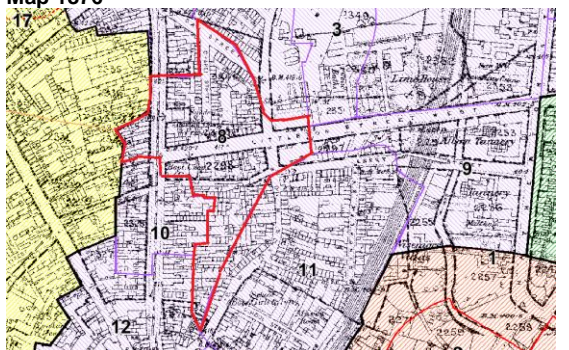
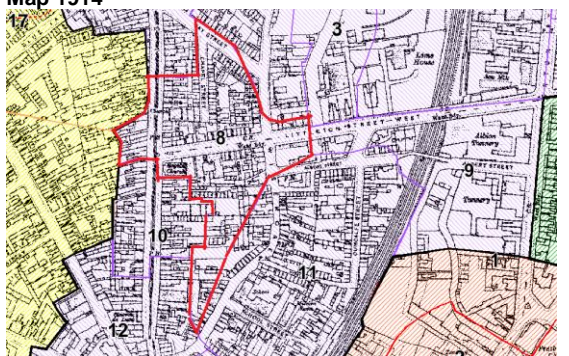
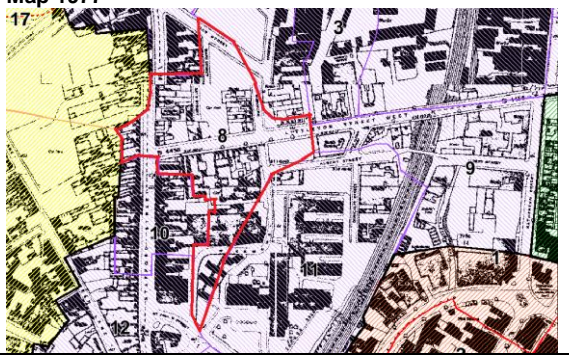
Capacity for change: There would be no resistance to undertaking changes to this site subject to design quality. The scale of the building is as high as could be tolerated in this location.

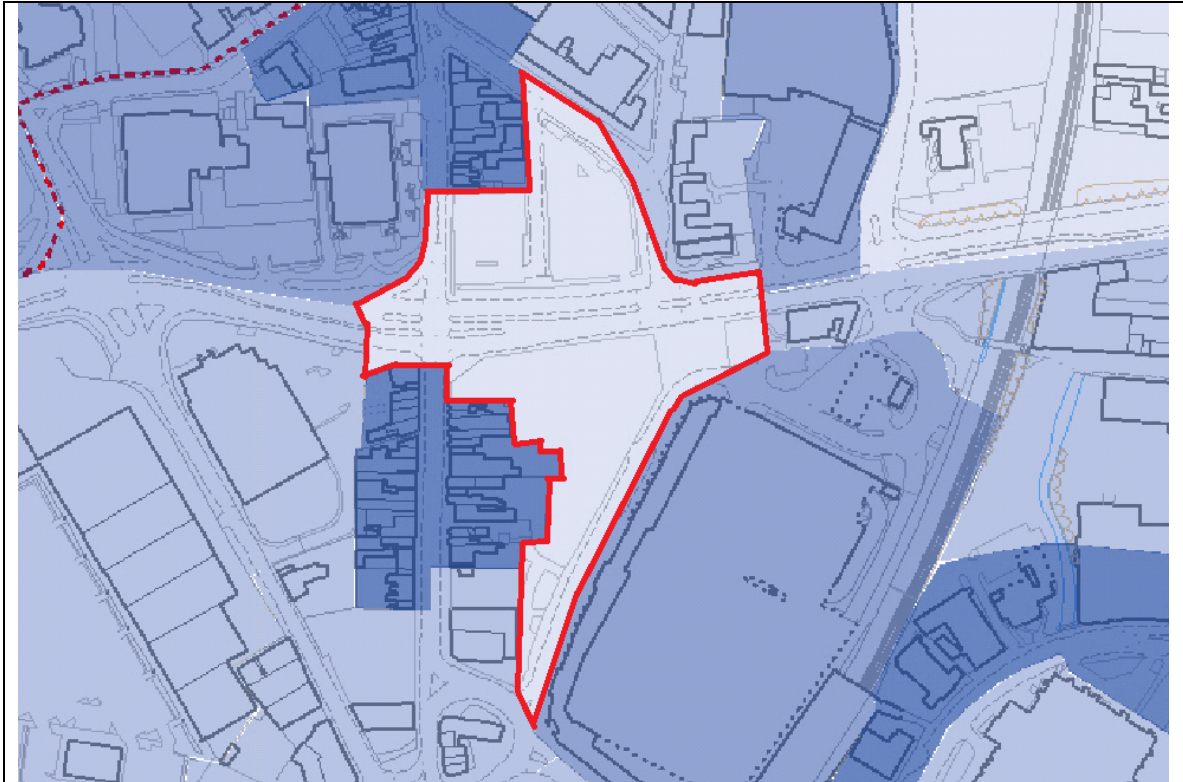


Opportunities: There are no current opportunities in this area.

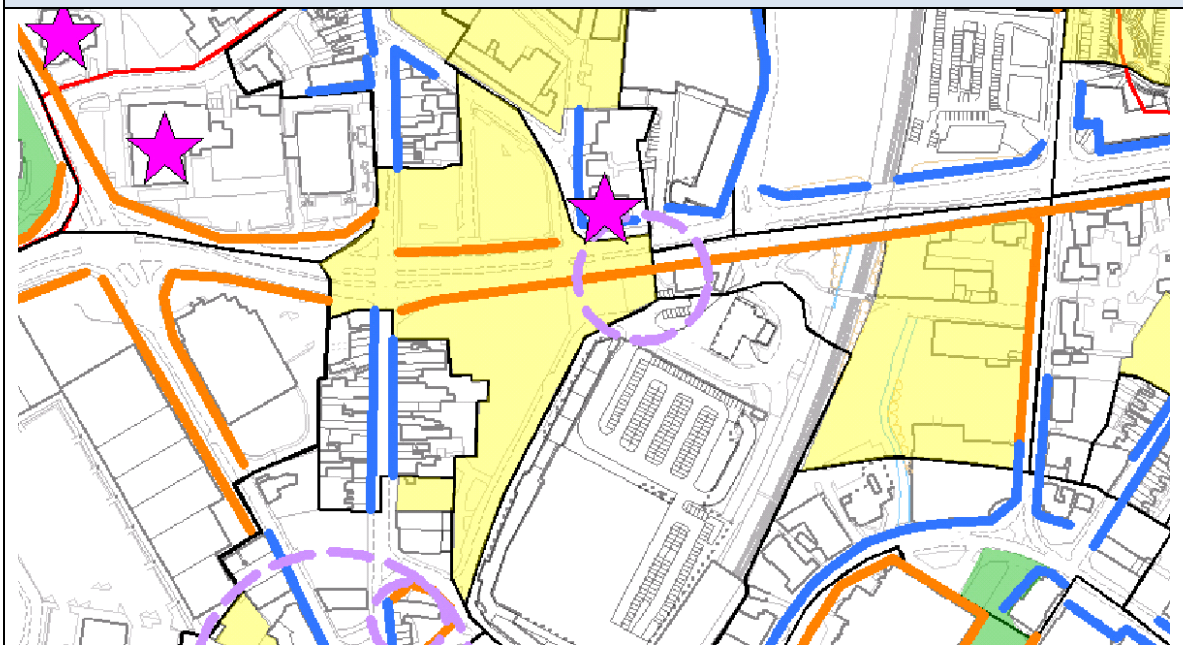
4.2.2.8 Littleton Street

<p>Character area: Town End – Littleton Street</p>	<p>Sub character area ref: TE8</p>
<p>HUCA: OP4</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>An area of cleared land and infrastructure along the northern edge of the town centre proper.</p> <p>The ring road runs east-west through the centre of this area, dividing a surface level car park (to the north) from an area of wasteland (to the south).</p> <p>The area is surrounded by streets and plots of land that are disparate in terms of the collective landscape. Whilst some adjacent areas comprise retail sheds and traditional buildings that overlook the area, there are also extensive frontages that comprise the rear aspect of buildings.</p>	
<p>Building types:</p> <p>There are no buildings within this area.</p>	
<p>Connections and urban grain:</p> <p>The area hosts a major traffic intersection (Stafford Street and Littleton Street West) which significantly divides the site in two.</p> <p>The northern side is a surface car park which is encircled by the busy traffic gyratory that flows round Day Street and back into Littleton Street West.</p> <p>To the south is the newly laid out street in front of the superstore which offers a controlled and pedestrian friendly environment.</p>	
<p>Uses:</p> <p>Surface car parking and wasteland.</p>	

<p>Predominant materials:</p> <p>Tarmac, granite sets and concrete flags.</p>	
<p>Positive characteristics:</p> <p>A number of established trees and some improved highway linkages.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>Dominated by large highway routes, junctions and gyratories that make pedestrian movement slow and difficult.</p> <p>A significant section of the ring road where there is no development, with the characteristics of a piecemeal planning process.</p> <p>Significant frontages of the area comprise the rear of buildings and their domestic servicing arrangements.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>A major double frontage onto the ring road where there is no enclosure on either side of the street.</p>	<p>Map 1977</p> 
<p>Sensitivity: 5</p> <p>The area is absent of any buildings and exposes the rear elevations of buildings on adjacent sites, it is not sensitive to change.</p>	




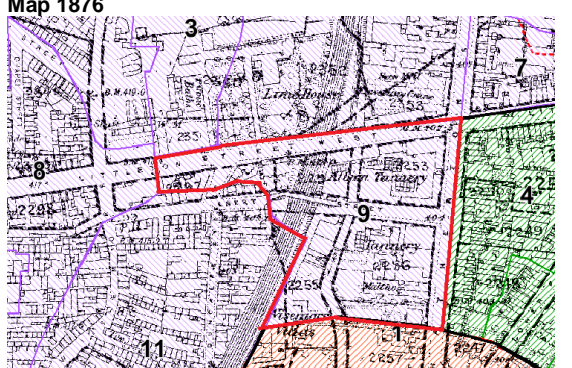
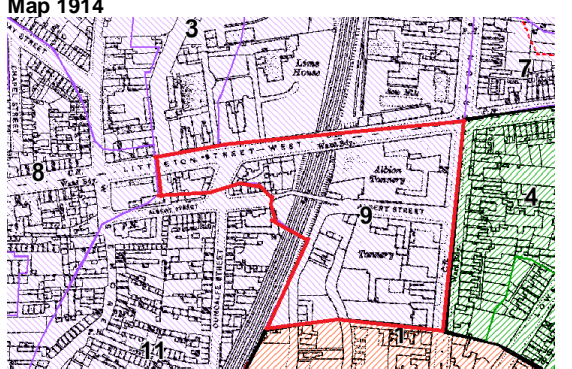
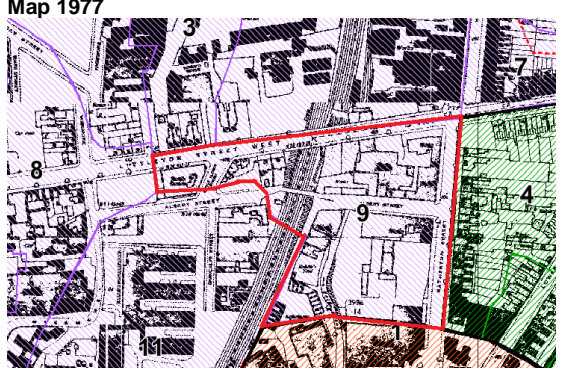
Capacity for change: There is a high capacity of change for this area, which would be positively encouraged. Built form of up to three-storeys in height would help screen off the rear aspect of Stafford Street (both north and south of the ring road). Buildings would need to front onto the ring road forming a strong enclosure of this primary route.

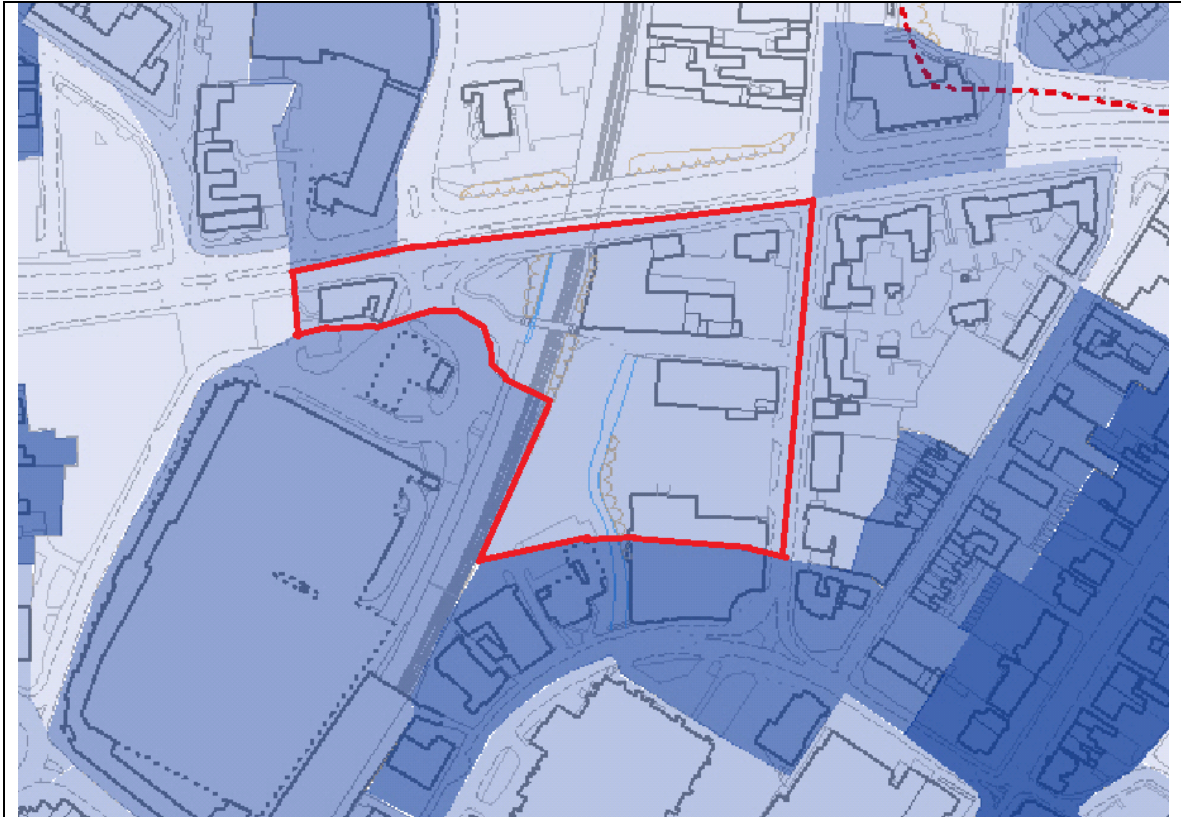


Opportunities: There are opportunities to develop the area either side of the ring road, with frontages through to Stafford Street (to the south) and through to the Wismore area to the north. Opportunities to increase connectivity across the ring road should be taken to link the town centre proper with the college and wider 'Gigaport'.

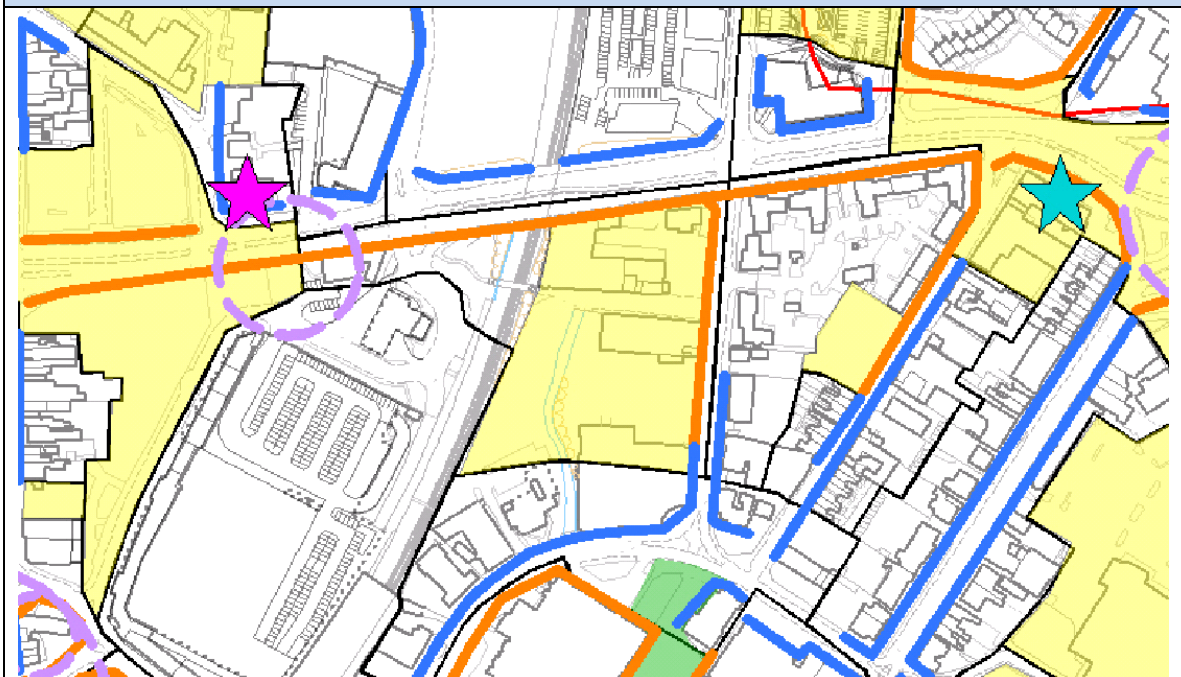
4.2.2.9 Albert Street

<p>Character area: Town End – Albert Street</p> <p>HUCA: IN2</p>	<p>Sub character area ref: TE9</p> <p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>A complex section of townscape sat between the southern side of the northern stretch of the ring road, a superstore development and 19th century industrial development.</p> <p>The area comprises an ad hoc collection of buildings around enclosed yards and open spaces and is subdivided by a brook, railway line and key pedestrian link.</p> <p>The area links to the landscaping around the superstore (to the south) due to an area of open landscaping and the mixed character of the development along Hatherton Street (to the east).</p>	
<p>Building types:</p> <p>A variety of 19th century commercial buildings, some of industrial character and others of finer architectural merit.</p> <p>The workshops are of utilitarian construction with simple arrangements of cambered arched windows in rows and lines. The office building fronting onto Littleton Street West has rubbed and moulded brick detailing applied to an asymmetrical plan and a profile defined by large half dormer second floor windows.</p> <p>Other 20th century sheds occupy the ring road frontage to the east and are comprise little more than clad frames.</p>	
<p>Connections and urban grain:</p> <p>The urban grain of the area is complex consisting of a hard frontage to the ring road or development and walling screening a highly organic network of plots behind.</p> <p>At a lower level are the cut of the brook (that becomes a culvert to the north) and railway line, but above is an ad hoc arrangement of building of a typical unplanned 19th century landscape.</p> <p>A pedestrian bridge connects over the railway offering an informal link between Albert Street (an unmade road) and the superstore site.</p>	
<p>Uses:</p> <p>Mainly light industrial, servicing and trade counter uses.</p> <p>A significant proportion of the site is vacant and redundant of any use, comprising cleared land and some surface level car parking.</p>	

<p>Predominant materials:</p> <p>Brick is the predominant building material in the area, with arches and architectural detailing also formed of brick, suggesting the low order of buildings in this area.</p> <p>Roofs are clad in Welsh slate and windows are either cast iron or timber.</p> <p>Modern buildings are steel frame and clad in corrugated metal systems.</p>	
<p>Positive characteristics:</p> <p>A number of buildings relating to the leather industry have been retained and reflect the incremental development of industry in the town during the 19th century. These buildings reflect the local industrial vernacular in their modest scale and simple architecture.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>A significant proportion of buildings are lost or laid to waste. Dereliction is rife and following clearance sites are now used only as surface level car parking. With open railings that fail to enclose the street properly.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>The area has a significant length of frontage onto Littleton Street west, the northern section of the town's ring road.</p> <p>The area retains evidential value from the town's leather making industries during the 19th century.</p>	<p>Map 1977</p> 
<p>Sensitivity: 4</p> <p>The area retains a single late 19th century works building which boasts intended architectural design (likely due to its prominent location historically). This building, along with the Albion Tannery building on Albert Street increases sensitivity, however, much of the wider area is devoid of buildings and therefore lacks sensitivity.</p>	




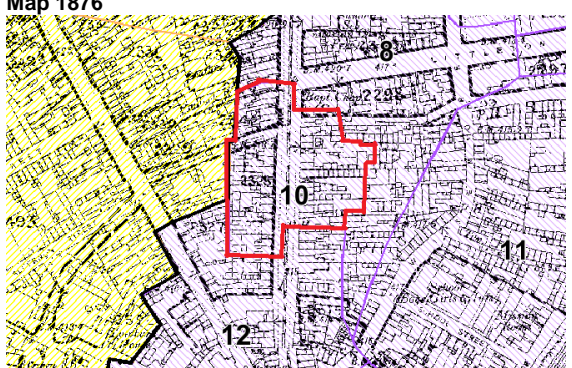
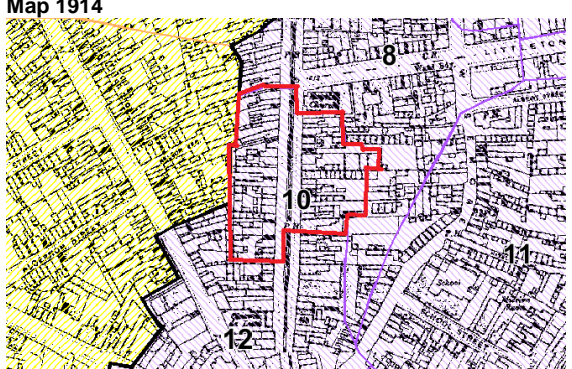

Capacity for change: There is capacity for change on the vacant site for buildings up to three-storeys in height, taking a cue from the historic buildings in the area. Strengthening street frontages will be an important consideration.



Opportunities: There are substantial opportunities for development across this area, including making use of the sites fronting onto Hatherton Road.

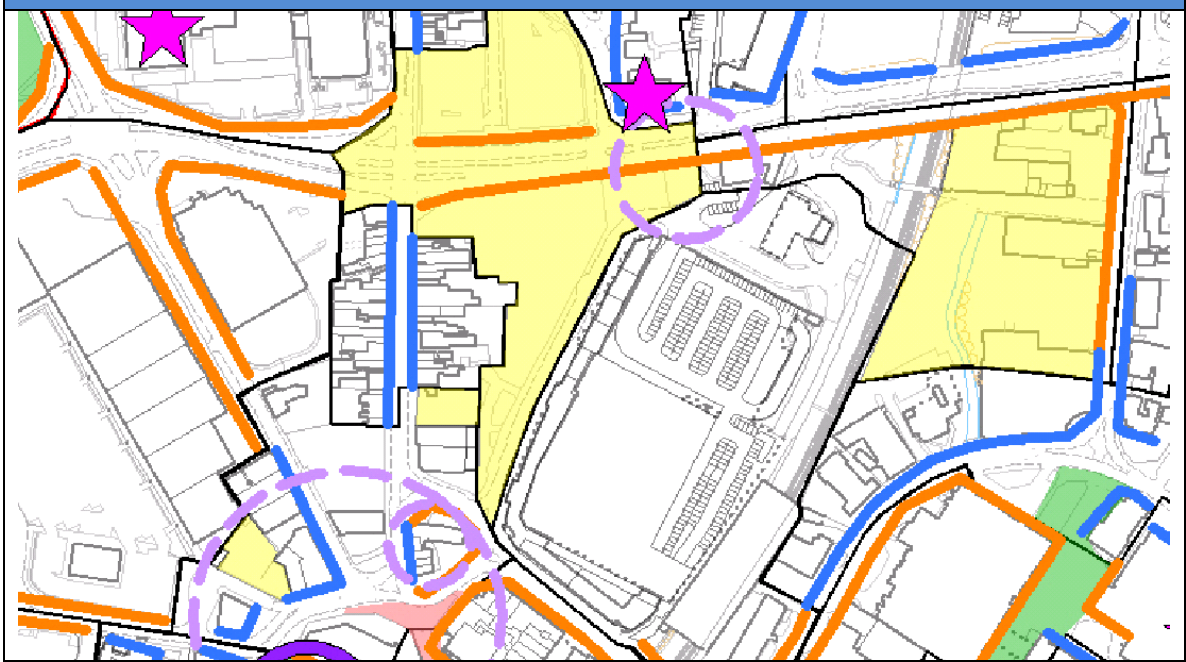
4.2.2.10 Stafford street (South)

<p>Character area: Town End – Stafford Street (South)</p>	<p>Sub character area ref: TE10</p>
<p>HUCA: RE2</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>An established area of 19th century development along an historic route linking Walsall to Bloxwich. The historic plot arrangements are largely intact and a wealth of mid Victorian two or three-storey shops survive.</p> <p>The character area has been significantly eroded as it passes north over Littleton Street West, but reinstates itself further up as it continues towards Bloxwich. The surrounding areas have been redeveloped throughout the 20th century and do not reflect this degree of completeness. Redevelopment of surrounding areas has been so complete that this area is very distinctive and appears as a hub of commercial activity.</p>	
<p>Building types:</p> <p>Buildings largely date from the 1850's to 1900 and predominantly comprise shops with accommodation above in the form of two or three-storey buildings with shop fronts at back of pavement and windows above set under console lintels, wedge lintels, flat arches and cambered arches. Roofs have side gables and eaves running parallel to the street.</p> <p>The late 19th century public house has an elevated grandeur that is emphasised with the brick pilasters, stone banding, oriel window and tripartite window with pediment.</p>	
<p>Connections and urban grain:</p> <p>Buildings are all sat at back of pavement and form strong street corridors in contrast to adjacent areas, where this deteriorates abruptly into open 20th century planning.</p> <p>Buildings are generally small and terraced so that the townscape is of a tight urban grain.</p> <p>All buildings front directly onto the street and there are few blind frontages. There are few open spaces and the character of the area is intimate and tight knit.</p> <p>Coach entrances were a feature now largely gone.</p>	
<p>Uses:</p> <p>The majority of uses are related to commercial activities including shops and fast food outlets and cafes.</p> <p>Upper floor are used as residential accommodation or not at all.</p>	

<p>Predominant materials:</p> <p>Brick dressed in stone is the predominant building material throughout the area and roofing is typically Welsh slate.</p> <p>Over the course of the 20th century some buildings have been rendered and even given a mock Tudor planted timber effect finish.</p> <p>Joinery details are sparse but constitute shop fronts and windows, although again these have frequently been replaced by modern materials such as UPVC plastic and powder coated aluminium.</p>	
<p>Positive characteristics:</p> <p>The survival and a significant proportion of original 19th century buildings allowing for the rhythm of the street to be retained through the repetitive sequence of small plots, either through terraced arrangement or sequence of individual structures.</p> <p>The consistency of the townscape through the construction of a single building type, punctuated by occasional more decorative building.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The deterioration of the areas condition through poor building maintenance, poor alterations and adaptations, poor changes of use, gaudy paint finishes and excessive use of poor quality signage and security measures.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>An area of highly typical 19th century development in the Black Country where plots housing shops with accommodation over form a community within a street.</p> <p>The architectural detailing of buildings is also highly localised and represents the evolution of terrace buildings across the Black country with the elevated eaves, wide spacing of windows and window lintels supported by consoles.</p>	<p>Map 1977</p> 
<p>Sensitivity: 2</p> <p>The area retains its historic layout and much of its 19th century development and therefore is relatively sensitive, however, as buildings are in poor condition and have been insensitively altered the sensitivity is lessened to some degree.</p>	

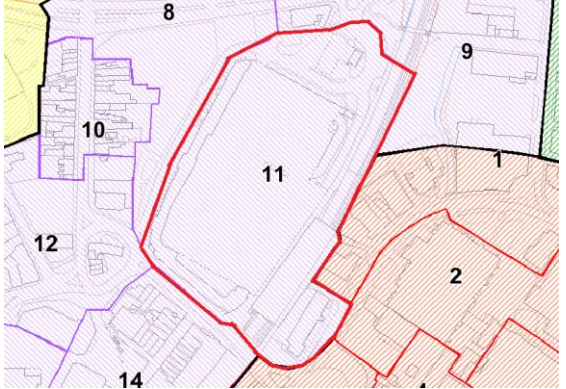






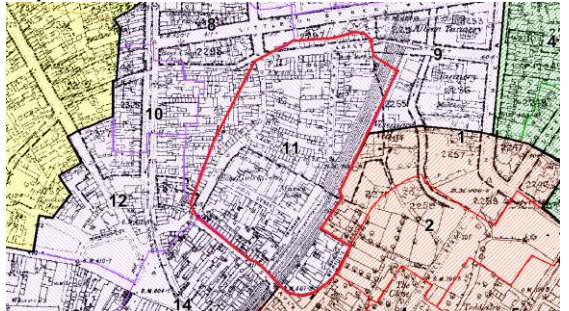
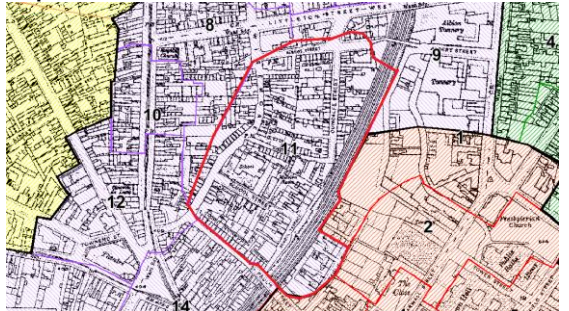

Capacity for change: There is limited capacity for change, which is largely restricted to alterations to existing buildings, or the replacement of individual buildings if the design is considered to outweigh the existing 19th century character of the area.

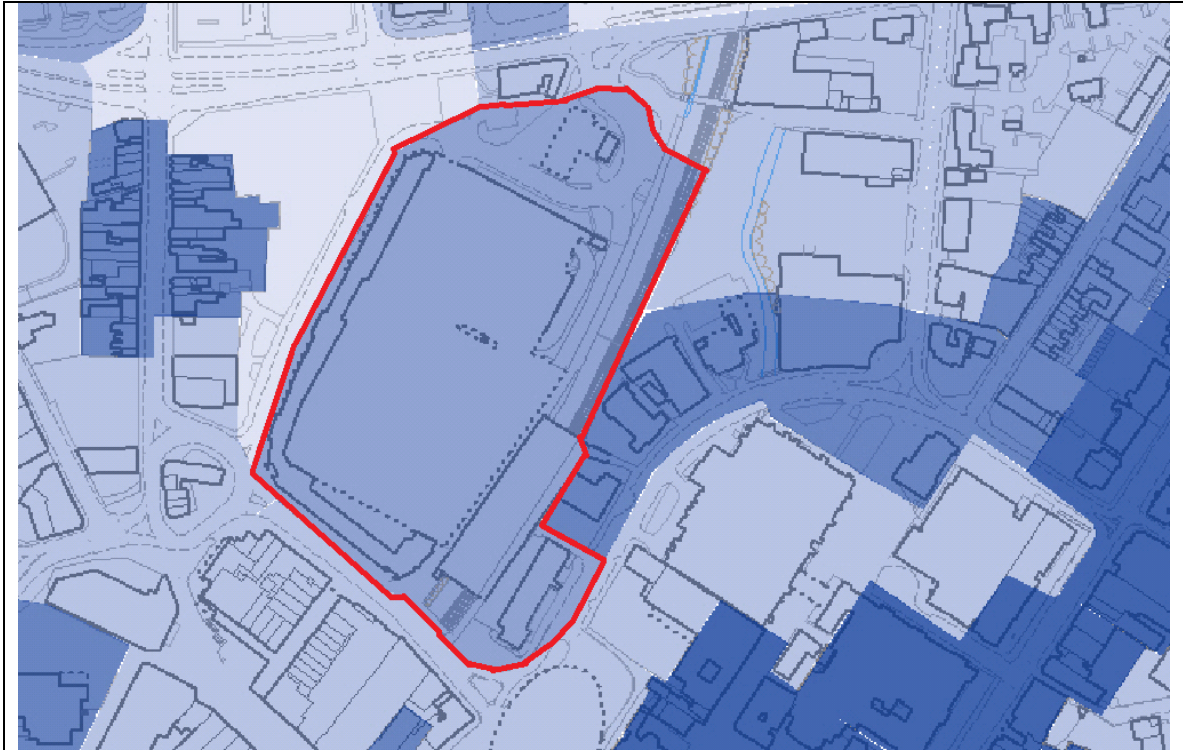


Opportunities: There are no opportunities in this area.

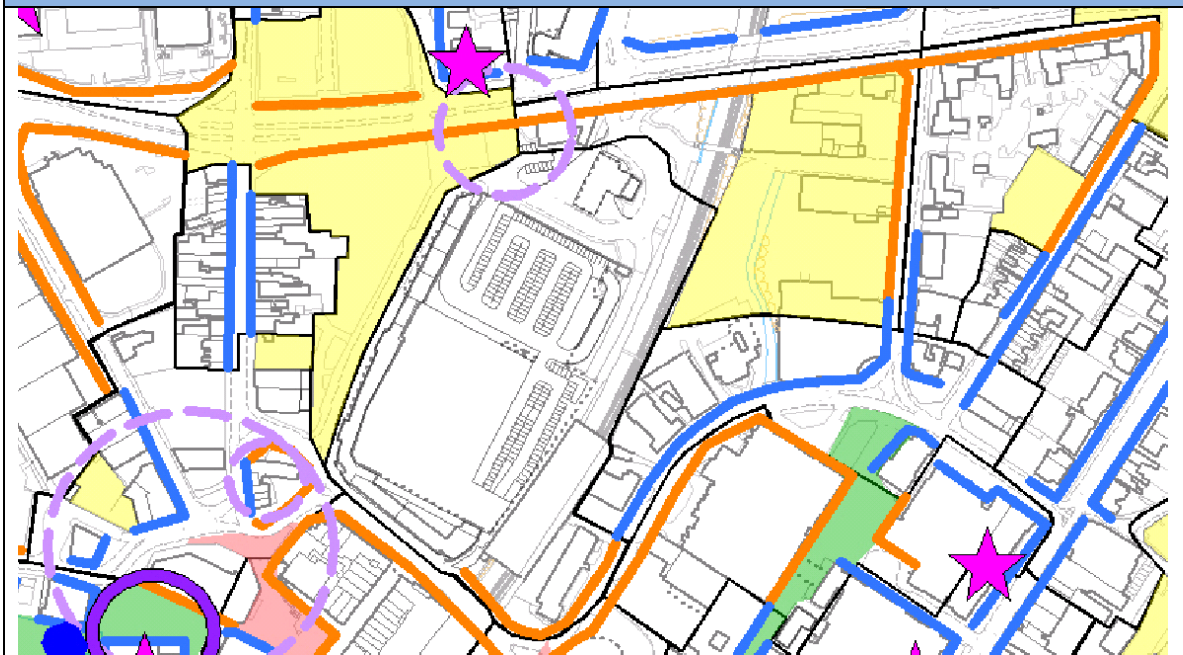
4.2.2.11 St. Paul's Street

<p>Character area: Town End – St. Paul's Street</p>	<p>Sub character area ref: TE11</p>
<p>HUCA: RE6</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>A large area of post war and modern development that occupies land that straddles and abuts the western side of the railway line as it approaches the tunnel into the station.</p> <p>The superstore building (and associated commercial floorspace) is the largest building footprint in the town centre and dominates a large town block.</p> <p>The area is a cluster of poorly conceived structures with limited architectural quality. Together the buildings and associated open spaces are incongruous and poorly related to the wider town. This is accentuated by the waste land to the northwest, the dereliction to the north east and similarly bland architecture to the south west.</p>	
<p>Building types:</p> <p>Three buildings in total:</p> <p>Multi storey Car Park: A series of decks linked by ramping and screened by open concrete struts.</p> <p>Office: A 1970's two-storey building with simple panel openings with expressed curved staircase well.</p> <p>Superstore and ancillary floor space: A modern clad shed with a series of aspects to break down its form. A framed brise soleil is used across the southern aspect in conjunction with arcading.</p>	
<p>Connections and urban grain:</p> <p>The buildings are large and are divorced from any reference to historic urban grain, being in part a section of a post war plan that sought to separate different modes of transport. A significant portion of the ground floor of this area comprises parking and a number of ramps and vehicular access dominate the street scene.</p> <p>On the St. Paul's street frontage (on the approach towards the bus station) the bridge over the railway line sits at back of pavement prior to a vacant plot on the corner with Hatherton Road. Voids and significant areas of open land are common as 'left-over' space in post war planning.</p>	
<p>Uses:</p> <p>A mix of retail uses, supplemented by officer floor space. Independent and allocated parking is a major land use in this area and is the use that has the most significant impact on the character of the area.</p>	

<p>Predominant materials:</p> <p>A wide use of materials are used in this area. In the post war structures, reinforced concrete in exposed floor plates and struts is employed along with brown and red pressed bricks.</p> <p>The modern shed has been dressed using a variety of materials in an attempt to break down its massing. A polished aggregate blockwork is used on the southern elevation, whilst terracotta rainscreen is used across the west facing elevation.</p>	
<p>Positive characteristics:</p> <p>The new superstore and associated units along the western elevation form an animated elevation towards the newly reformed Wisemore Street and screen the railway line that runs through the site.</p> <p>The new shed has been dressed with higher quality materials than is normally not seen on such development and has been applied in is such a way as to create a sense of individuality across the wider elevation.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The architecture of the post war structures is poor quality with poor quality and failing materials.</p> <p>The modern sheds comprise contrived architecture applied to a shed to counter its scale rather than achieve good architecture.</p> <p>A number of incremental open spaces that are not managed or maintained, but do not comprise developable parcels of land.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>A large and dominant site, which is a destination independent of the town centre proper, much in the same way as Crown Wharf is.</p>	<p>Map 1977</p> 
<p>Sensitivity: 3</p> <p>The area has been comprehensively redeveloped with a retail shed, save for the sunken railway line and 1970's office building along the eastern boundary, whilst some design has been concentrated into the existing development, its retention is not critical to the future regeneration of the town, limiting sensitivity considerably.</p>	

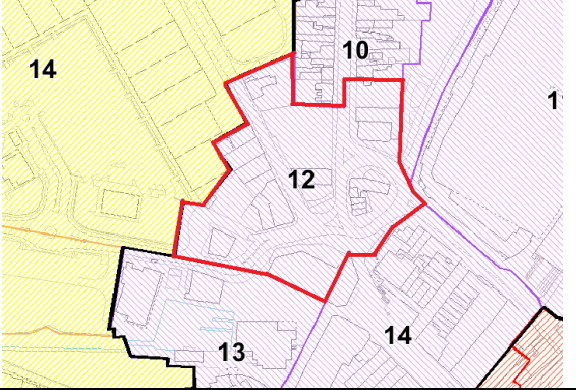






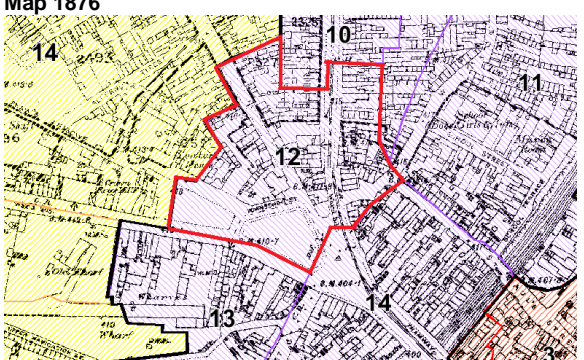
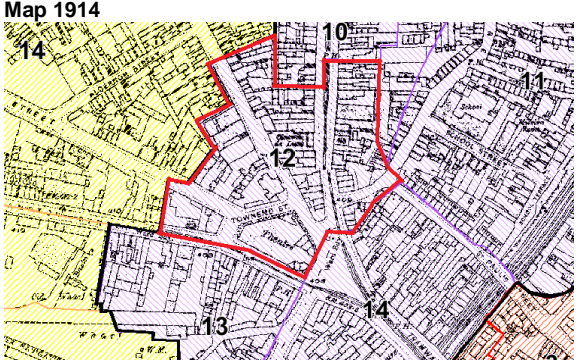
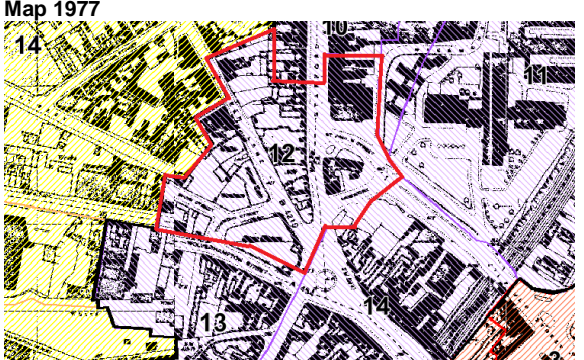
Capacity for change: The capacity for change is high and reflects the acceptability for further (substantial) redevelopment, however the scale and density of development would unlikely increase substantially.

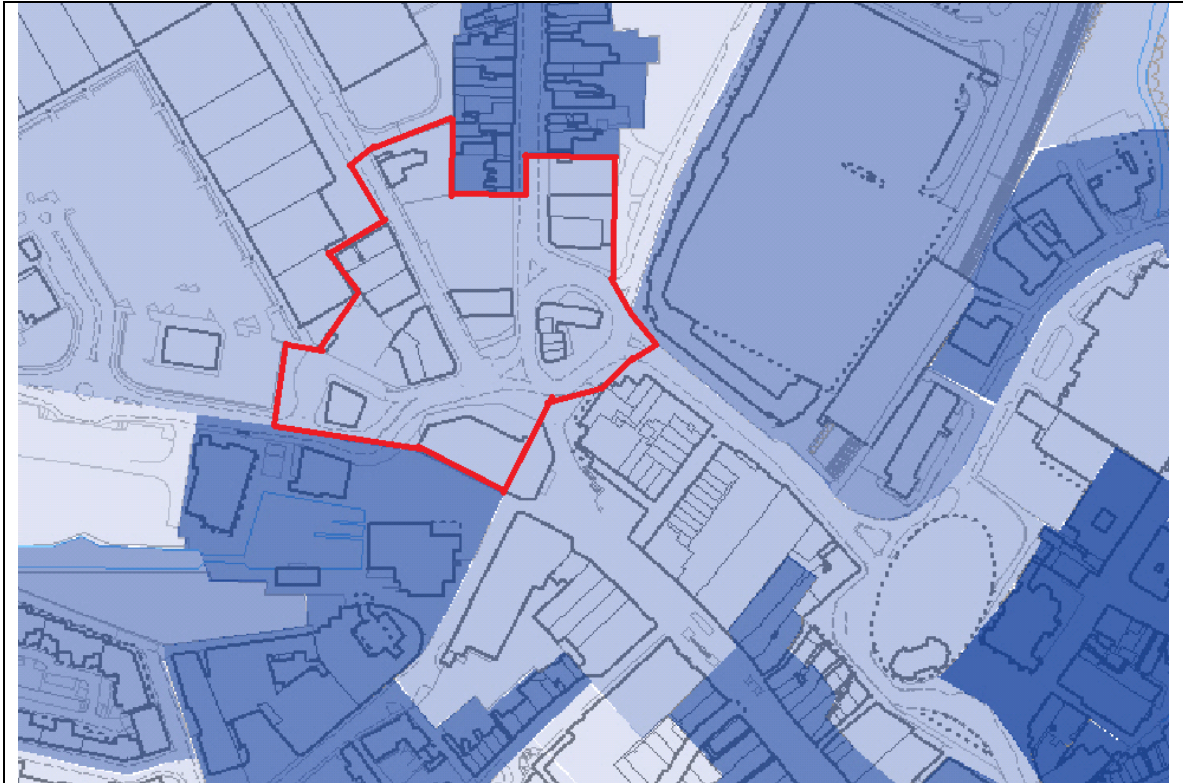


Opportunities: There are limited opportunities for development although an opportunity to improve connectivity across the ring road along the historic Wismore route should be encouraged.

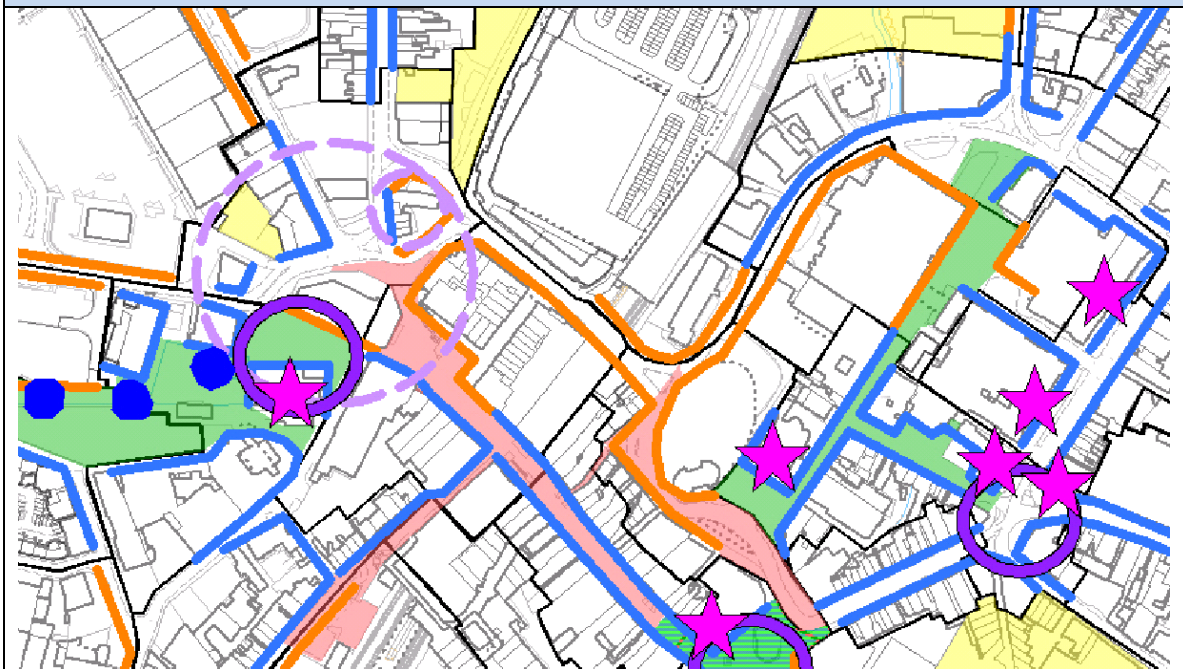
4.2.2.12 Townend

<p>Character area: Town End - Townend</p>	<p>Sub character area ref: TE12</p>
<p>HUCA: RE2</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>An area centred on the confluence of six roads leading into the northern end of the town, comprising a roundabout with wedges of land surrounding it.</p> <p>Remnants of a now largely demolished 19th century townscape survive, but these are integrated with areas of surface level parking and cleared land.</p> <p>There is no dominant architectural character or scale to built form, but rather the roads themselves are the dominant characteristic.</p>	
<p>Building types:</p> <p>A mix of mid 19th century three-storey independent commercial premises, mixed with larger early 20th century warehouse and showroom structures.</p> <p>Meeting halls and pubs are also present and further add diversity to the scale of built form along the street.</p> <p>Architecture ranges from classical, to late Victorian eclectic styles, stripped classism and Tudor Revival.</p>	
<p>Connections and urban grain:</p> <p>The roundabout has been formed through the historic form of the southern termination of Stafford Street, which has resulted in a parade of shops isolated on this roundabout, with their rear elevation completely exposed. Selective clearance of buildings elsewhere has had a similar impact and now party walls and rear elevation and service areas are exposed to passing traffic which approaches it from a variety of angles.</p>	
<p>Uses:</p> <p>A variety of retail, fast food, cafe and public houses dominate the character of this area along with surface level parking.</p>	

<p>Predominant materials:</p> <p>Brick remains the dominant material in the area, however other materials such as render, faience, terracotta, moulded plaster and planted timber feature heavily, either independently or in conjunction with this primary building material.</p> <p>Most roofing is clad in Welsh slate or concrete tiles.</p> <p>Metal, timber and plastic windows are common.</p>	
<p>Positive characteristics:</p> <p>A number of exceptional individual buildings survive, including the early 20th century steel framed faience clad building on the corner of Green Lane and Townsend Street.</p> <p>The group of buildings along the west side of Green lane comprise a group of good quality buildings from the early 20th century in a standard setting at back of pavement and not affected by poor highway engineering works of the post war planning in the town..</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The formation of the roundabout that has demolished significant parts of the southern end of Stafford Street and isolated a section of the street within the middle of the junction.</p> <p>Selective demolition of site that has deteriorated the overall composition of the townscape opening up views to the rear elevations of buildings and resulted in the formation of surface level parking.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>The area comprises the termination to the thoroughfare that is the High Street / Park Street retail corridor.</p> <p>The area is a main arrival point into the town centre proper from the north.</p>	<p>Map 1977</p> 
<p>Sensitivity: 4</p> <p>The area comprises the ruminants of the late 19th and early 20th century, retaining a number of buildings, but also containing a number of sites hosting lost buildings and a substantial piece of highway infrastructure, isolating a small group of historic buildings in the middle of a gyratory system. The historic townscape is still detectable, however has been seriously compromised.</p>	

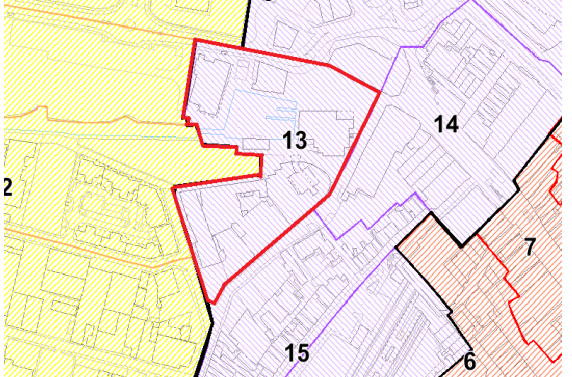





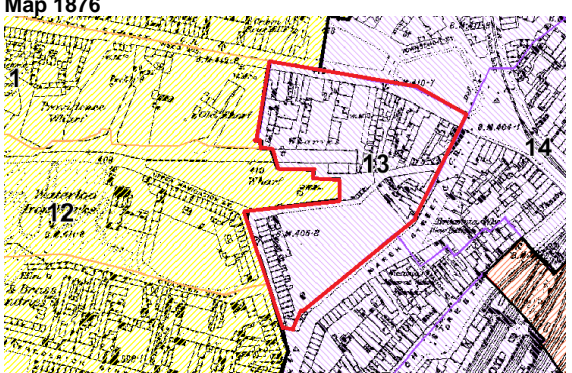
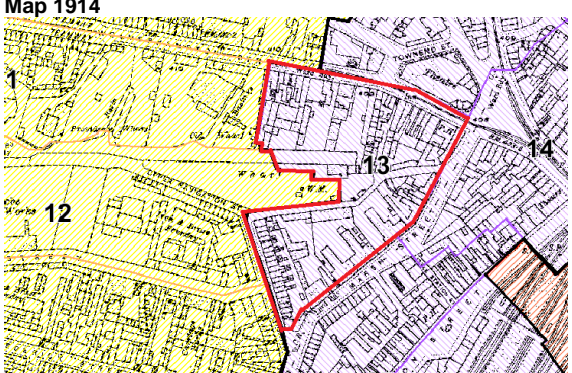
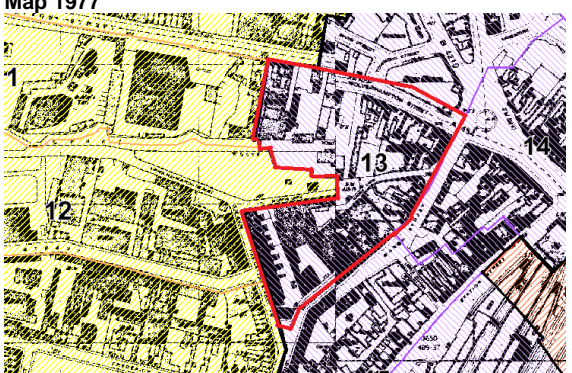
Capacity for change: Considering that the area sits at a confluence of roads and varying character areas and relates to none of them there is substantial capacity for change, however this should not be at the expense of historic buildings that survive.

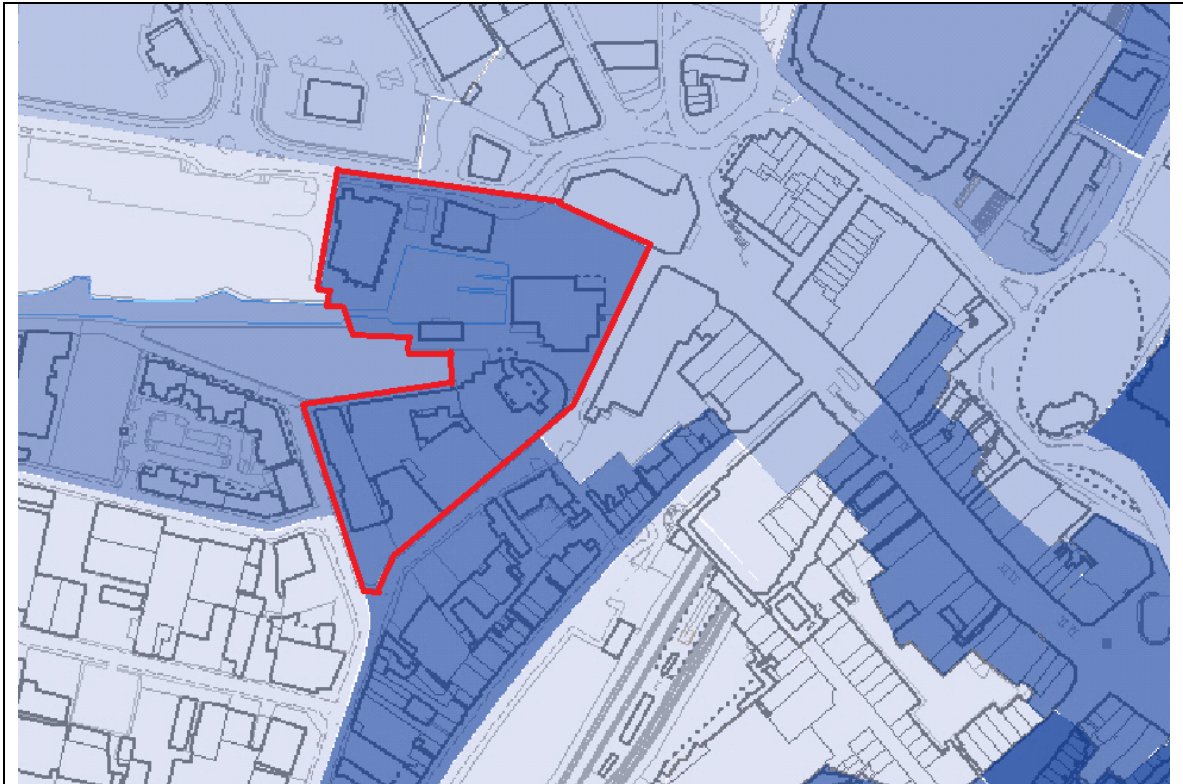


Opportunities: There are a number of small opportunity sites with the capacity to extend up to three or four storeys in height (depending on the immediate context). The main opportunity to be sought is to strengthen this area as a node and increase connectivity from the town centre along Wismore and Stafford Street as well as into Crown Wharf.

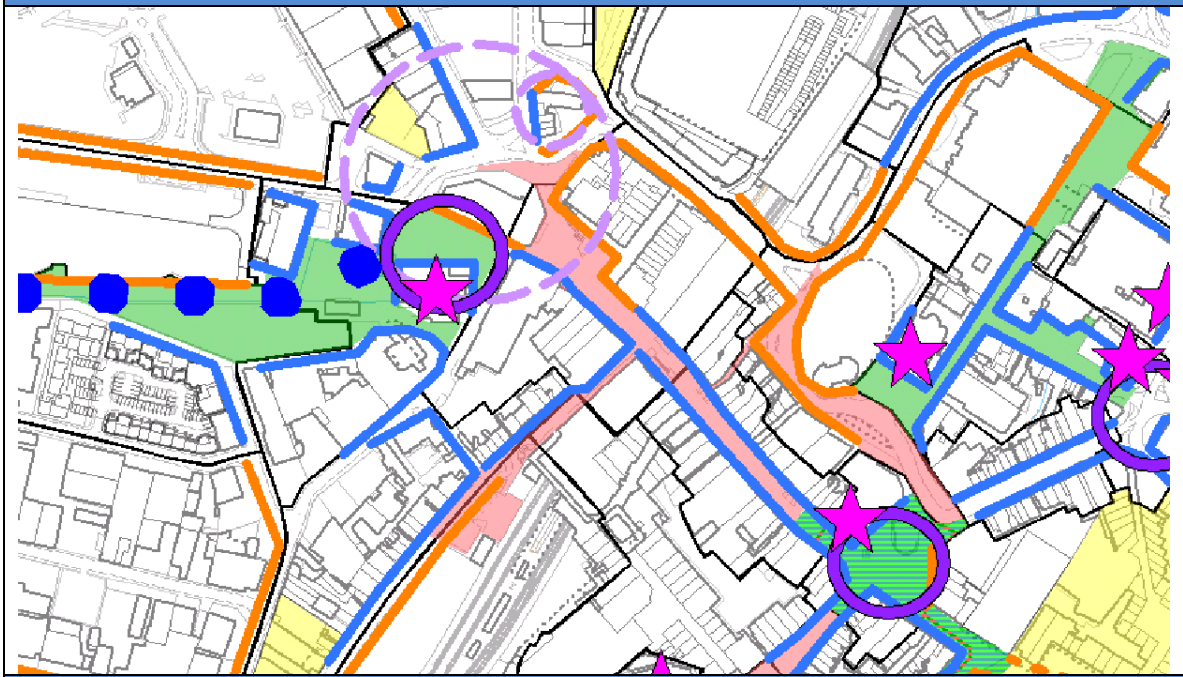
4.2.2.13 Art Gallery

<p>Character area: Town End – Art Gallery</p>	<p>Sub character area ref: TE13</p>
<p>HUCA: MX2</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>The termination of the Walsall Canal (Town Branch) as it spurs into the town and terminates at the wharfs surrounding the canal basin. Today the area is the new cultural destination within the town heralded by the construction of the 'New Art Gallery' along with new hotel, apartments, cafes and restaurants.</p> <p>The area is directly adjacent to Town End, the historic termination of the town and is linked to the end of the main high street of the town, but also the area to the south and west where historically industry developed in association with the canal and later the railway.</p>	
<p>Building types:</p> <p>The area retains some historic works buildings as well as finer office buildings dating to the 19th century. The buildings are mostly constructed along simple utilitarian forms with rows and lines of segmental windows. Historic office buildings incorporate pilasters and entablatures between floors. Wharf buildings have canopies over the water body with openings on the same aspect to facilitate trade.</p> <p>Modern buildings employ robust abstract architectural form and comprise substantial height to mark out this new destination.</p>	
<p>Connections and urban grain:</p> <p>The water-space comprises a basin surrounded by hard landscaping up to its edges. This hard landscaping extends between buildings (covering significant tracks of land and reflects the industrial character of the canal.</p> <p>Buildings are large and have been arranged around the water-space to enclose it. Links have been formed through the public realm to link through to Park Street and round the wharf building to connect to the canal tow path.</p> <p>Buildings close down the frontage towards Townsend Street.</p>	
<p>Uses:</p> <p>The area has the greatest diversity of uses in the town centre. There are a host of leisure and cultural uses including the art gallery, hotels, bars and cafes, but also a significant group of residential apartments that form part of the new residential community that has developed along the southern side of the canal.</p>	

<p>Predominant materials:</p> <p>The historic building stock is constructed in a pallet of red brick, with higher order buildings dressed in moulded red brick or terracotta, and works beings occasionally being dressed in bands of either blue or purple brick.</p> <p>Modern buildings have employed a diverse range of cladding solutions, including grey terracotta and steel on the art gallery, dark grey render, gloss panelling and coloured framing pods on the hotel, timber and steel on the apartments and timber planking on the wharf bar.</p>	
<p>Positive characteristics:</p> <p>The grouping together of a number of high quality modern buildings that together with retained 19^m century commercial buildings has created a dynamic townscape of juxtaposing compositions.</p> <p>The formation of spaces that connect with the water-space and link though to existing centres of activity. These have successfully drawn pedestrians through this new area as they pass between Crown Wharf and the town centre proper.</p> <p>A vibrant mix of uses supporting a day and night-time economy.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>A number of the historic buildings, whilst retained, lay redundant and derelict. Similarly some of the ground floor spaces under apartment buildings have not seen commercial take-up.</p> <p>Links across the can are poor and this has resulted in the south side of the basin struggling to see the same degree of investment as the northern side.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>An important part of the West Midland canal network as it extends into one of the regions town centres providing it with the opportunity to interact with the wider townscape it passes through.</p> <p>The canal basin is a significant reference point within the town and provides a prominent link between the town and its industrial past.</p> <p>The formation of a new cultural centre that is part of the towns new legacy.</p>	<p>Map 1977</p> 
<p>Sensitivity: 2</p> <p>The area contains a number of substantial historic commercial and works buildings as well as wharf buildings and also a variety of modern new build development including the art gallery, apartments, bars and hotels. It comprises a rare example in the town of where new and old architecture coexist successfully and a modern townscape has emerged with great merit in a canal setting.</p>	

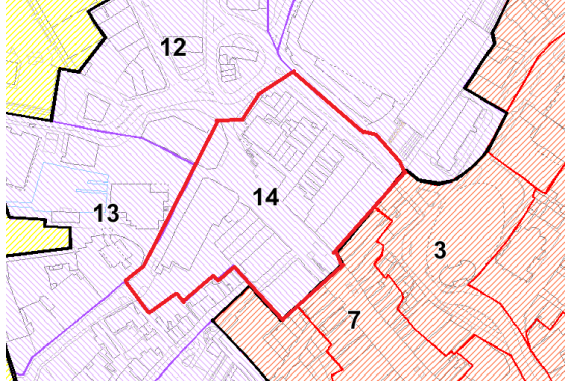






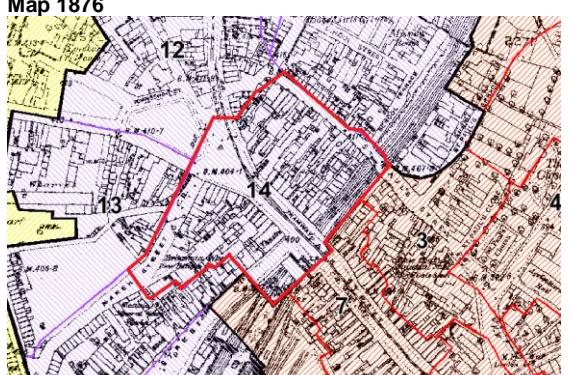
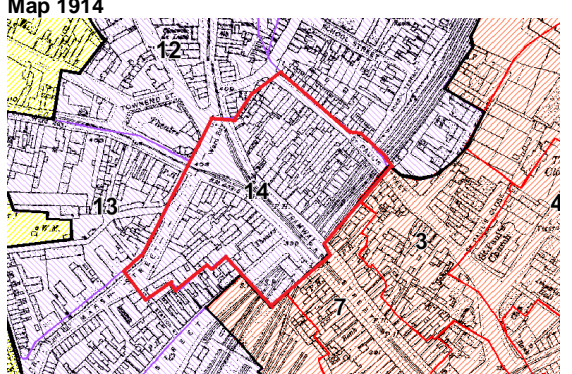
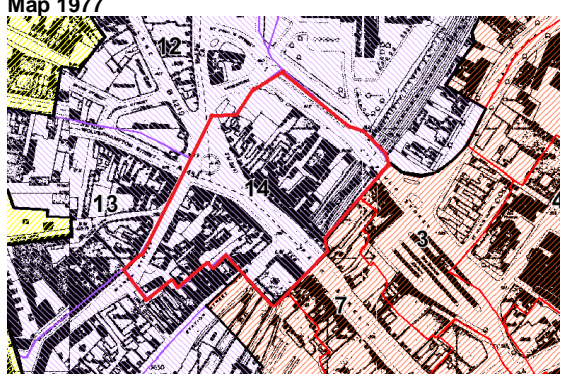
Capacity for change: The area appears as having been through contemporary transition and therefore a capacity for change exists, but is balanced against the need retain much of the built form present. Should individual site need to be radically redeveloped, new design needs to better that to be lost and reflect the scale of historic development.

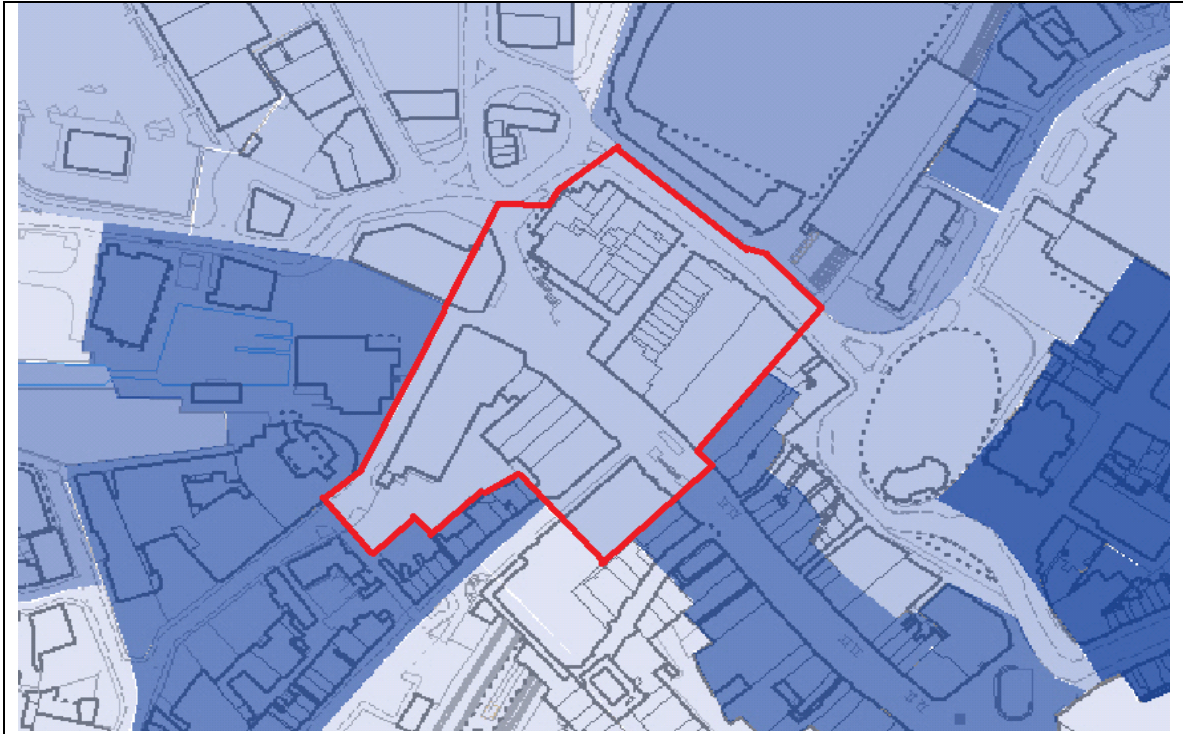


Opportunities: Opportunities present themselves in the conversion of buildings along the southern side of the canal.

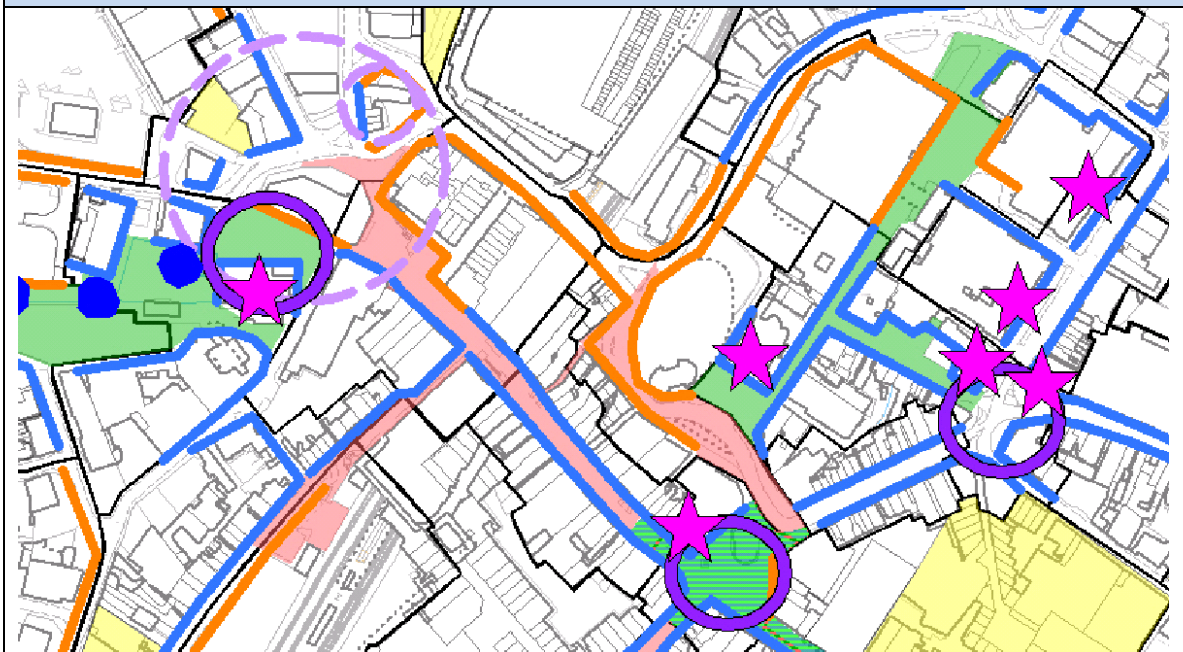
4.2.2.14 Park Street (West)

<p>Character area: Town End – Park Street (West)</p> <p>HUCA: RE2</p>	<p>Sub character area ref: TE14</p> <p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>The area comprises the north-western half of the Park Street area and differs from that of the south-eastern half as the area has been significantly redeveloped from the post war period onwards.</p> <p>The area splits towards its terminus and leads into both Townend (to the right) and the New Art Gallery and canal basin (to the left).</p> <p>The historic organic sweep of the street is still evident and although this is disrupted by some of the building form, the overall street layout is retained.</p>	
<p>Building types:</p> <p>The area comprises a significant group of 1960's buildings along its northern side and post modern buildings along its southern side.</p> <p>The 60's structures comprise a sheer relationship between a horizontal mall structure with expanses of solid masonry and a tall commercial tower with bands of windows.</p> <p>The post modern development delivers a series of deconstructed gables with circular turrets with a baronial influence and large arched shop front entrance.</p>	
<p>Connections and urban grain:</p> <p>The historic alignment of High Street, through Digbeth The Bridge and on down Park Street is retained and the flow of the street continues on into the historic terminus that is Townend.</p> <p>The buildings that define this space contribute little to preserving this urban grain and are at odds with the subtle organic form of the public realm. Structures are large and cumbersome, bearing no relationship to historic plot forms and obliterating any rhythm to the street scene.</p>	
<p>Uses:</p> <p>The area forms part of the principal retail area of the town and comprises shops, cafes and fast food outlets.</p> <p>The tower offers office floorspace.</p>	

<p>Predominant materials:</p> <p>Brick and concrete are the primary building materials in this area of the town. The 1960's development utilises a brown brick with expressed concrete floor plates, whilst the post modern development made a far greater play on materials using red and blue pressed brick in bands and dressing the opening and coping with concrete sections.</p> <p>Paving is red block paving and concrete flags applied along post modernist principles and is a contemporary of this latter phase of retail construction.</p>	
<p>Positive characteristics:</p> <p>The area has a high foot fall and clearly benefits from its links with the leisure activities that have established around the canal basin and Crown Wharf (to the north).</p> <p>The Red Lion public house is an important historic building that has survived the redevelopment plans in this area and is one of the best examples of late 19th century gin palace architecture from the period in the town. Its eclectic mix of Jacobethan detailing and Flemish gabbling is unrivalled in Walsall.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The brutalism of the 1960's building is not a well executed work, it employs poor quality materials, introduces large sections of 'dead' frontage and has been significantly compromised by later reworking diluting its confident original design.</p> <p>The post modernist development shamelessly mimics the historic vernacular of the town and constitutes a contrived piece of sham architecture in a regrettable prominent location within the town.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>A significant track of the high street corridor, connecting the main shopping area to Townend and the canal area and New Art Gallery.</p>	<p>Map 1977</p> 
<p>Sensitivity: 4</p> <p>The area has been radically redeveloped over the last 50 years and has lost all its historic built form other than the Red Lion public house, the layout of the street and the form of the public realm as the northern end of the high street terminates survives and remains important.</p>	




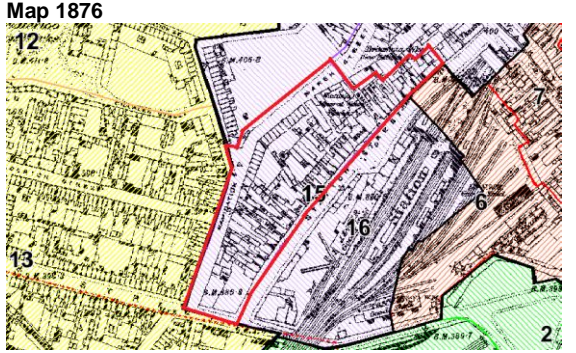
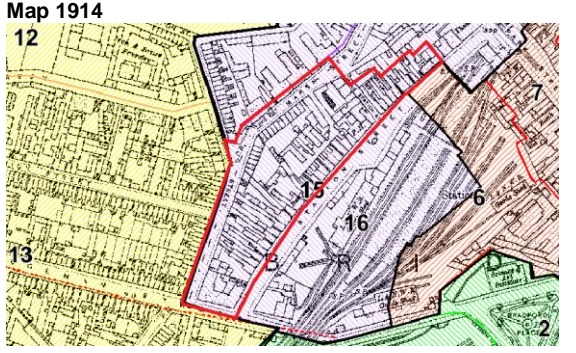
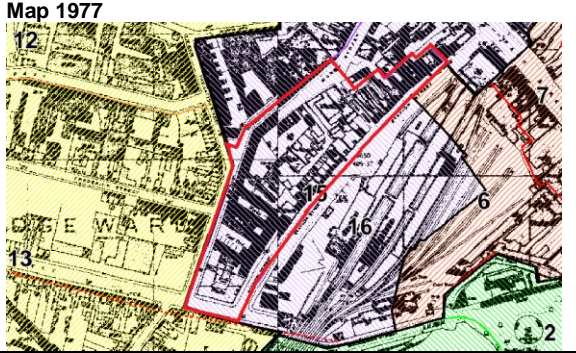
Capacity for change: There is substantial capacity for change and the redevelopment of most of the building (other than the Red Lion) would be welcomed. The existing scale of development is acceptable along the southern side of the street and the head of the street, but comprehensive redevelopment of the northern side would likely see a lowering of building heights and a strengthening of the historic organic form of the building line.

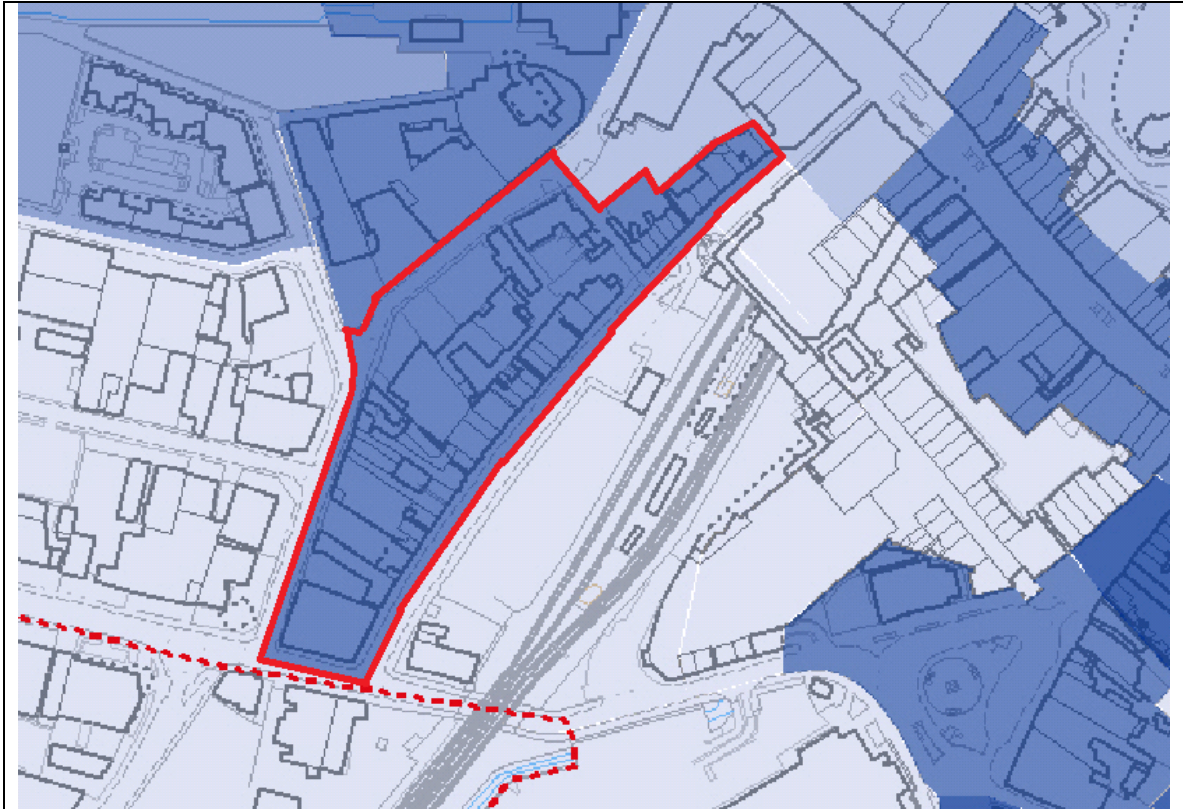


Opportunities: Redevelopment opportunities are limited, although parcels of land are significant and less incremental than in other parts of the town. Opportunities to improve connectivity to Stafford and Wisemore Street as well as Crown Wharf and the canal are important as well as any opportunities to redress the existing postmodern buildings to the south of the street and head of the street which lack maturity in design.

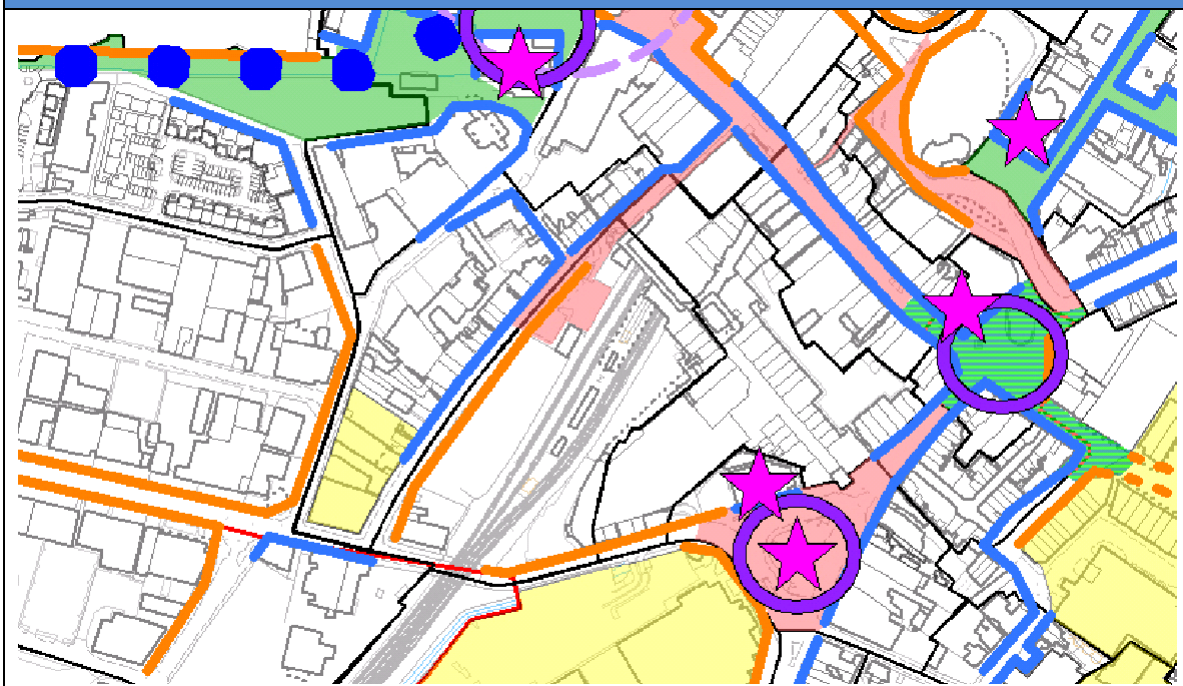
4.2.2.15 Station Street

<p>Character area: Town End – Station Street</p>	<p>Sub character area ref: TE15</p>
<p>HUCA: RS2</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>A long linear area that fronts the industrial district south of the canal. The area comprises a mid 19th century terrace of housing that overlooks what was the main entrance into the railway station, delivering a degree of civic pride.</p> <p>The rear of these buildings comprise workshops that illustrate the intimate association of residential and industrial activity across the Black Country.</p> <p>The area is a southern spur leading off of the main shopping corridor and is now divorced from this area since buildings were constructed over the railway line.</p>	
<p>Building types:</p> <p>A series of terraces built over a short period of time, comprising three-storey town houses.</p> <p>The architecture of the buildings is a local take on mid 19th century town houses, using heavy lintels over doors and windows supported on console brackets. The ground floor comprises a base, separated from the upper floors with heavy banding or weightier architectural treatment.</p> <p>The workshops to the rear vary in size from three storey structures that extend the full depth of the block to single cell structures within a yard.</p>	
<p>Connections and urban grain:</p> <p>The linear form of the block reflects the civic need to present a confident and respectable frontage towards the station. Pedestrian links to exist through the terrace, although principal access to these units would likely be from the street to the rear.</p> <p>The terrace represents the careful handling of Georgian and Victorian town house architecture, which allows for grand set-piece buildings to be constructed that still employ the rhythm of individual plots.</p> <p>The loss of the works building on the corner with Bridgeman Street has significantly damaged the closure of this composition.</p>	
<p>Uses:</p> <p>The buildings in this area comprise shops, offices and crèches etc as well as residential use. The area also suffers from significant dereliction and vacancy, particularly towards the southern end adjacent to the lost works building on the corner with Bridgeman Street.</p>	

<p>Predominant materials:</p> <p>The terrace was originally a composition of red brick dressed in sandstone. This has been disrupted by the painting and rendering of elevations, that has masked architectural details and damaged the impact of the set-piece architecture that is so important to the overall composition.</p> <p>Roofs are slated, although not easily visible due to parapets and low pitches.</p> <p>Workshops to the rear are in matching materials, but often lack or have simplified stone dressing to reflect their lower order.</p>	
<p>Positive characteristics:</p> <p>The terraces still constitute a elegant arrival point into the town from the railway station, even if the station itself has been poorly redeveloped.</p> <p>The area constitutes one of the few planned parts of the town, where buildings collectively were designed to impress.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The loss of the building on the corner with Bridgeman Street has exposed the flank wall of the end terrace and the condition of this end of the street is now in serious decline with dereliction becoming a serious problem.</p> <p>The rendering of individual properties has disrupted the overall appearance of the street as has the replacement of windows, poor signage and other insensitive alterations.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>An important piece of 19th century planning to create a piece of civic townscape opposite the entrance to the station. The buildings are unravell'd in the town centre from this period and unlike the grand properties lining Bradford Street and Lichfield Street, are reflective of a Black Country typology in their detailing and proportions.</p> <p>The area contains important survivals of industrial workshops connected to the rear of housing in urban areas, illustrating a period when industrial activity was not fully separated from the home.</p>	<p>Map 1977</p> 
<p>Sensitivity: 2</p> <p>The area retains much of its historic urban grain and building stock, although this has been depleted at the southern end with the loss of an important listed building.</p>	

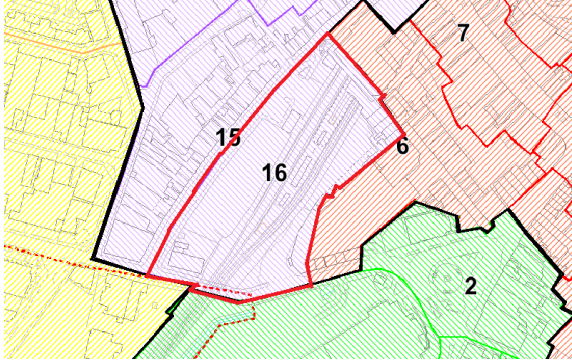






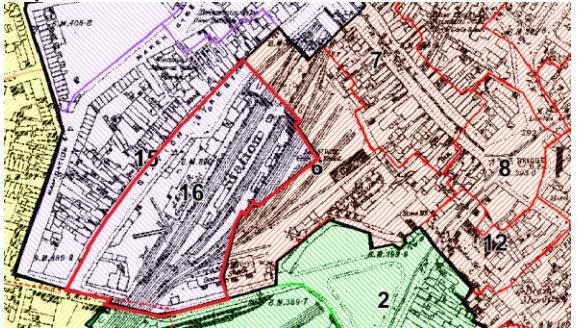
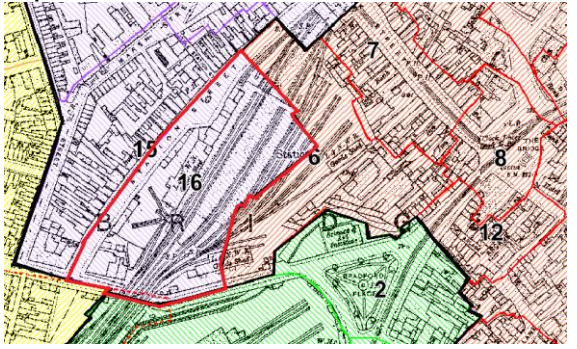
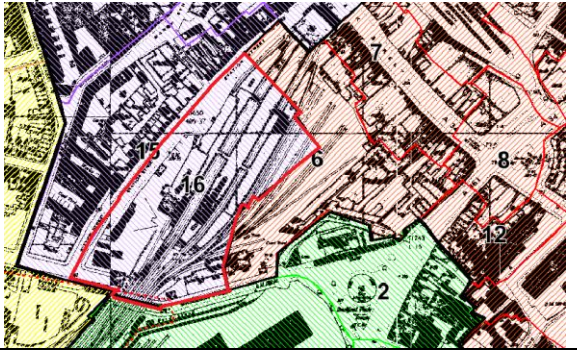
Capacity for change: There is limited capacity for change other than at the southern end of the area where there is now a vacant site on the corner with Bridgeman Street. This building could extend up to 4 storeys in height which would relate well to the wider terrace, yet mark the street corner well (in addition to its elevated base and falling topography).

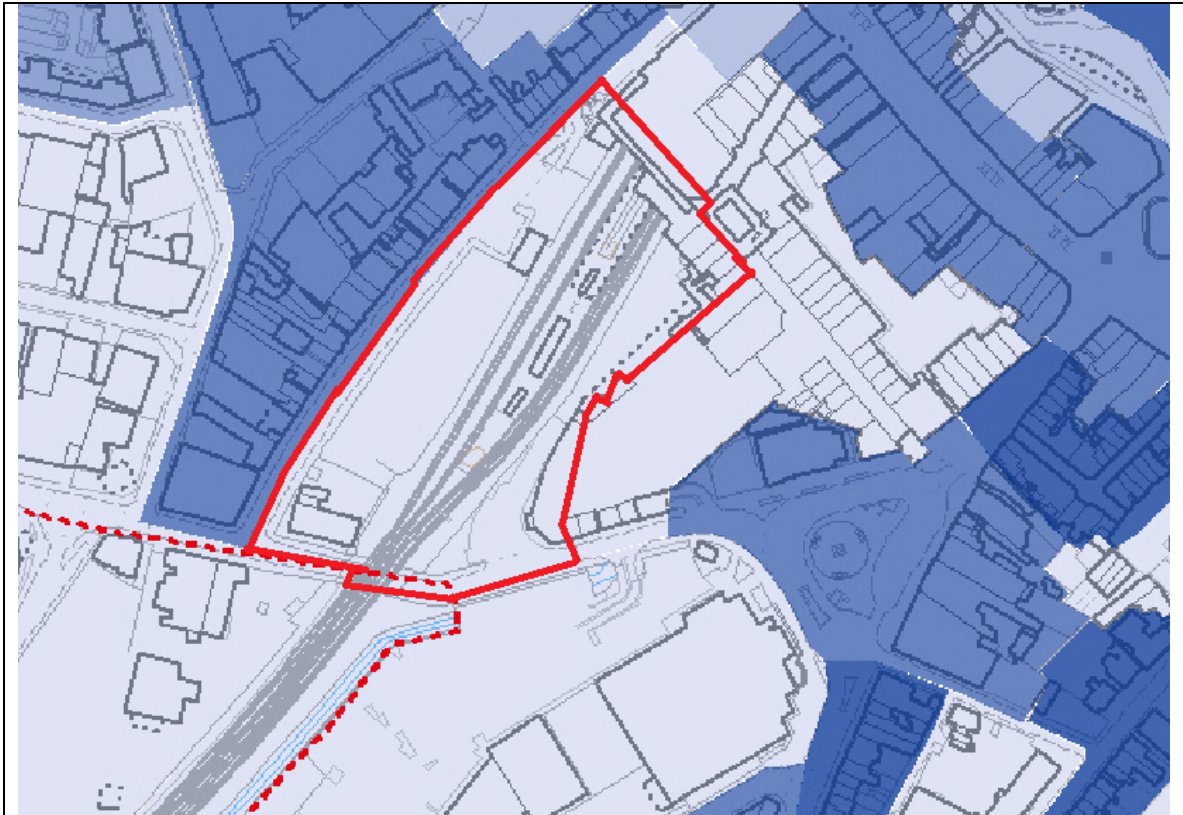


Opportunities: There site to the southern end of the area is cleared and could be developed comprehensively with a strong frontage towards the station.

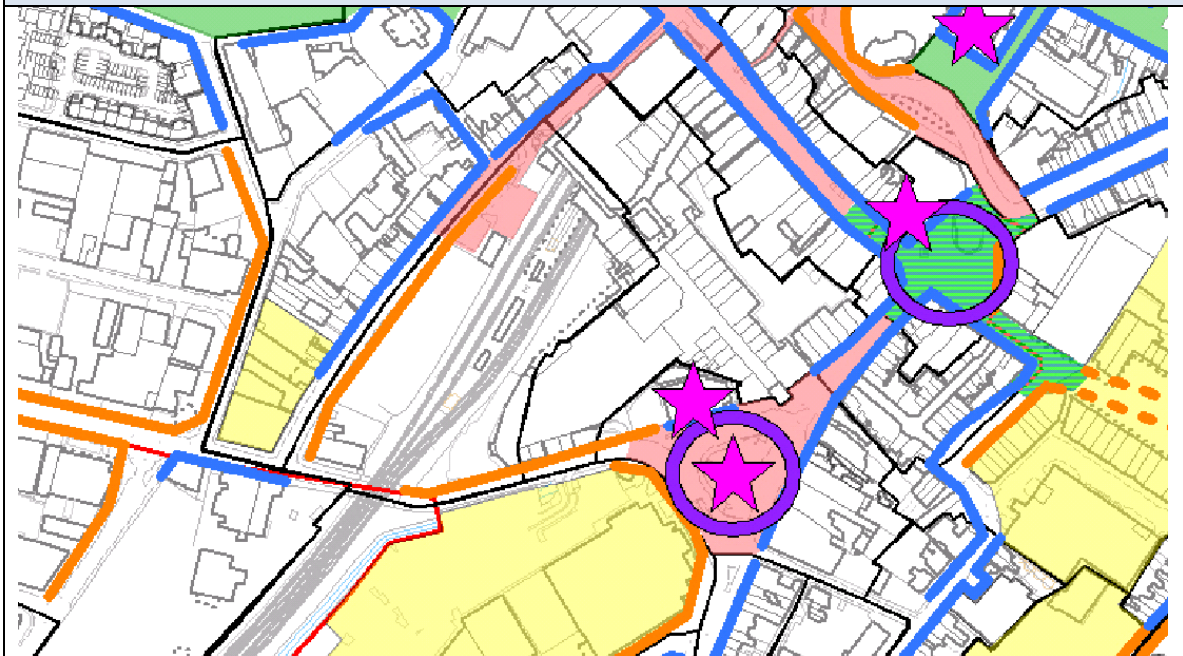
4.2.2.16 Station

<p>Character area: Town End - Station</p>	<p>Sub character area ref: TE16</p>
<p>HUCA: T2</p>	<p>Survey date: 9th July 2014</p>
<p>Overview and relationship to adjacent sub areas:</p> <p>The large track of land comprising the railway station platforms, sidings and adjacent car parking.</p> <p>The station building is now lost into the shopping mall that was developed over the former Park Street Bridge. And the historic frontage to the station has been lost, opening up the land beyond towards the Station Street terraces.</p> <p>The loss of the works on the corner of Bridgeman Street and Station Street further opens up views from this area to the industrial trading estate along Bridgeman Street.</p>	
<p>Building types:</p> <p>There are few free standing structures and the main building is the extensive rear façade of the Saddlers Shopping Centre which encloses the area from the east. This largely comprises three storey blank walling and an open decked multi-storey car park.</p> <p>Freestanding platform shelters, platform canopies and the Station Street ticket office all date to within the last 20 years and are entirely utilitarian.</p>	
<p>Connections and urban grain:</p> <p>Besides the railway lines, the area comprises levelled surfacing with car parking on it and areas of hard-standing in front of the station that function as a turning circle and vehicular access to the car park.</p> <p>The absence of any real built form has resulted in the area comprising no urban grain and with the reordering of the station (transferring most of its ticketing and staffing functions up into the shopping centre) the historic frontage is now devoid of any planned composition in the street scene.</p>	
<p>Uses:</p> <p>The area is entirely used as the train station or as associated storage and parking.</p>	

<p>Predominant materials:</p> <p>Modern brown and structural concrete are prominent building materials in this area, along with red brick paving and tarmac.</p>	
<p>Positive characteristics:</p> <p>The station is well used and comprises a major gateway into the town.</p> <p>Trees have established along the boundary with Station Street.</p>	<p>Map 1876</p> 
<p>Negative characteristics:</p> <p>The station is a very poor piece of transport infrastructure, it is accessed either through a shopping centre or down Station street, which appears as an unanimated side street from the main shopping area. The station, therefore, has poor legibility and is effectively hidden from the town centre proper. The form of the shopping mall entirely screens it and instead the building is isolated from other functions in the town.</p> <p>The Station street ticket office does not reflect the importance of its gateway function and both it and the other structures are of negative architectural value, being entirely utilitarian and disjointed in their relationship to one another and the surrounding areas.</p>	<p>Map 1914</p> 
<p>Statement of significance:</p> <p>A major arrival point into the town since the 19th century coming of the railway. The station is an important link in the towns public transport strategy and is a significant node of activity throughout the day.</p>	<p>Map 1977</p> 
<p>Sensitivity: 5</p> <p>The area comprises the historic alignment of the town's railway infrastructure but has lost all of its enclosures and station architecture, the service face of the shopping centre now dominates over the area and as such the area has no sensitivity.</p>	



Capacity for change: The capacity for change is high and welcomed. Development over the station car park to a scale of up to four-storeys is welcomed along with a redevelopment of the station to connect it through to the retail frontage of Park Street is important to improve this gateway.



Opportunities: There is a substantial opportunity to redevelop the site in conjunction with the transportational aspirations and constraints of Network Rail.

